

**A Strategic Plan for the
Restoration of the former
United States Coast Guard Station**

Cleveland, Ohio

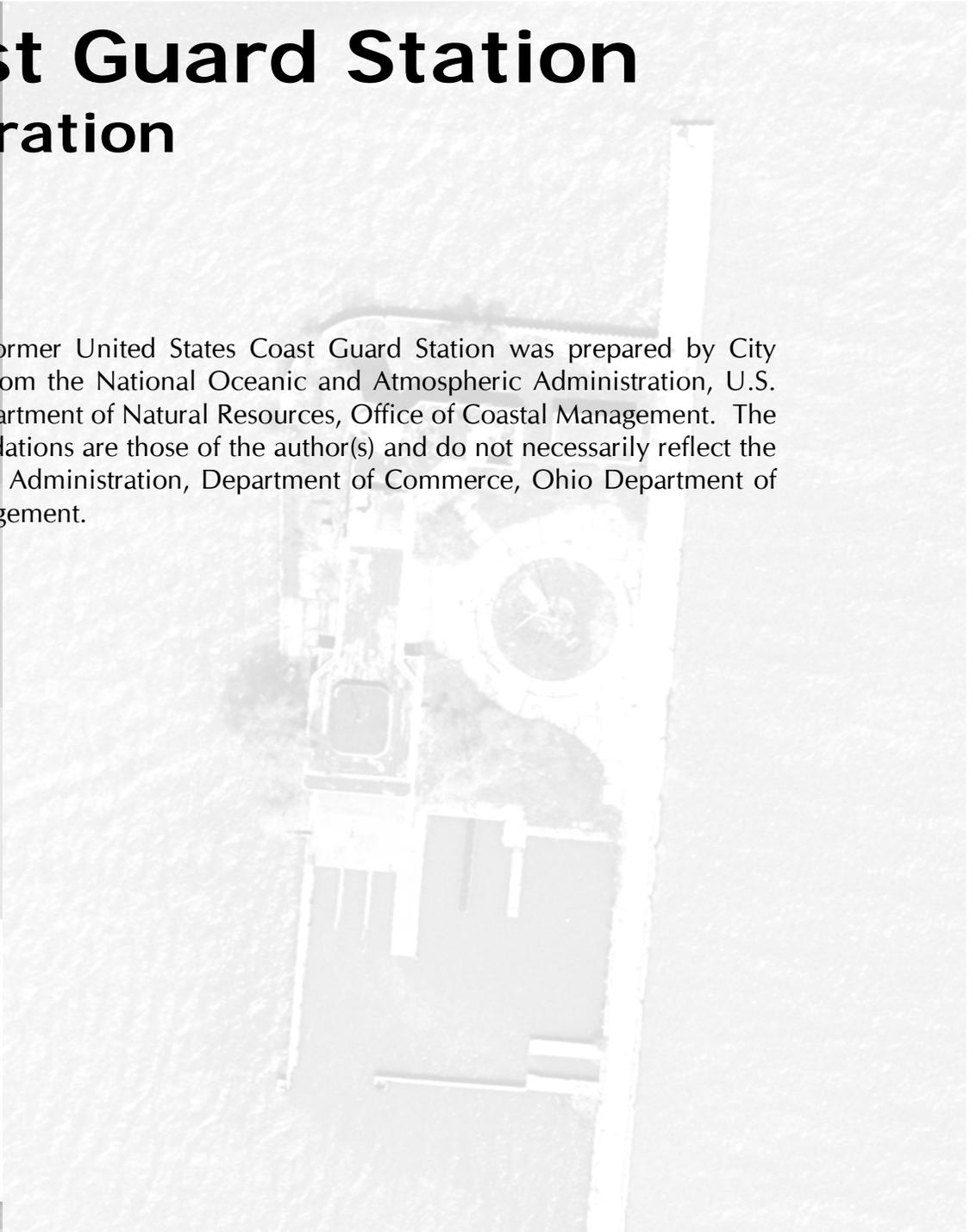
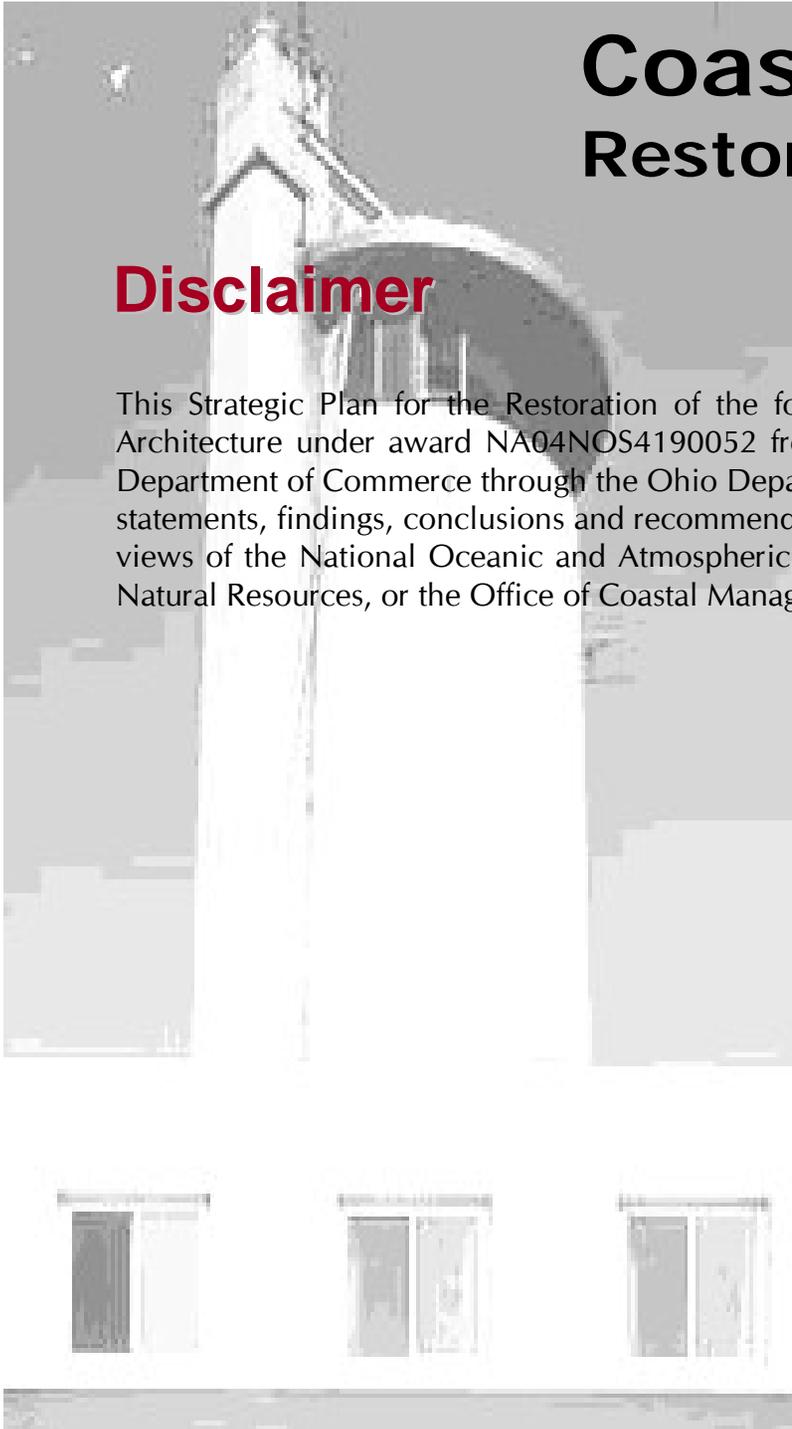
**City of Cleveland
cityarchitecture**

March 31, 2006

Coast Guard Station Restoration

Disclaimer

This Strategic Plan for the Restoration of the former United States Coast Guard Station was prepared by City Architecture under award NA04NOS4190052 from the National Oceanic and Atmospheric Administration, U.S. Department of Commerce through the Ohio Department of Natural Resources, Office of Coastal Management. The statements, findings, conclusions and recommendations are those of the author(s) and do not necessarily reflect the views of the National Oceanic and Atmospheric Administration, Department of Commerce, Ohio Department of Natural Resources, or the Office of Coastal Management.



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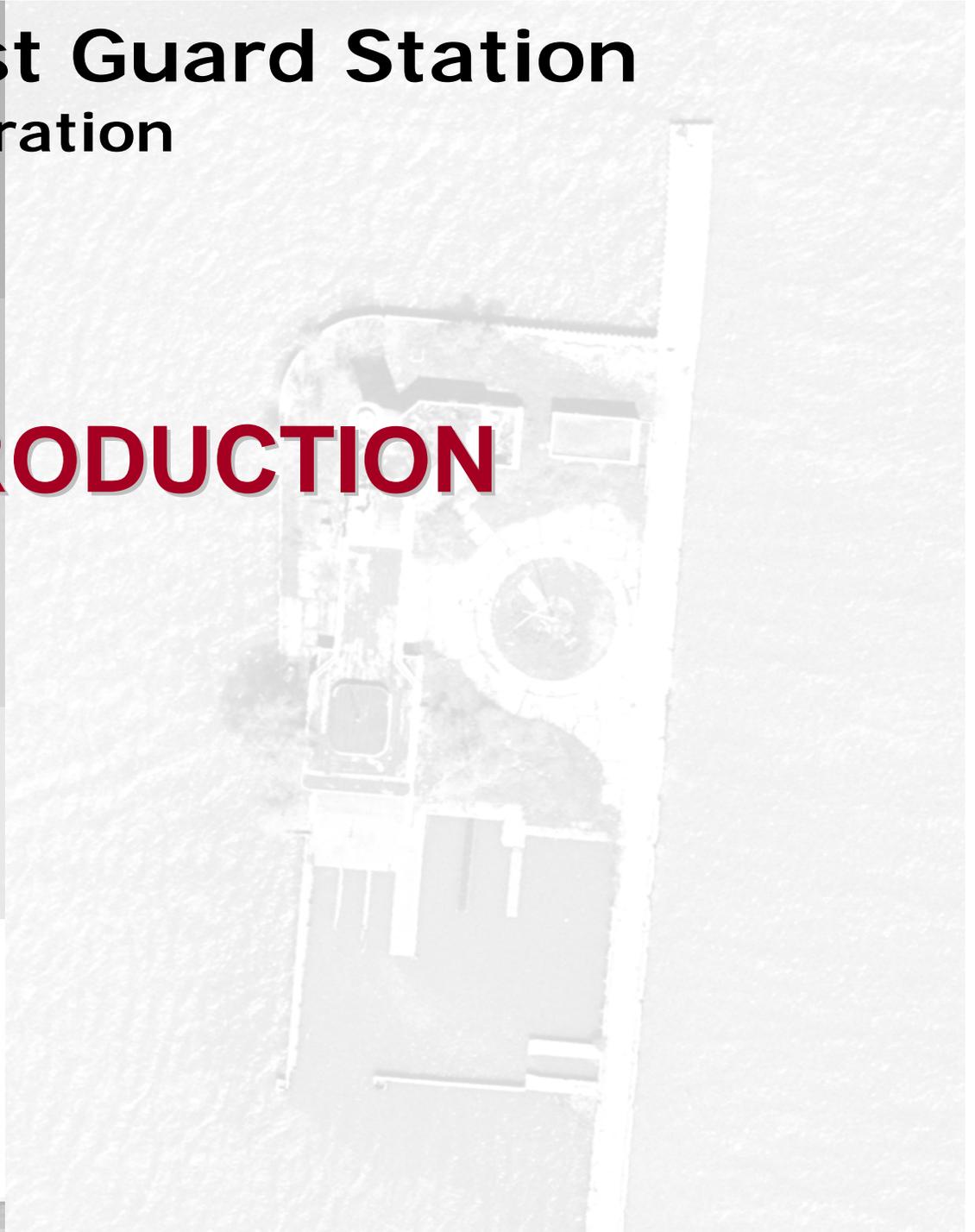
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Coast Guard Station Restoration

INTRODUCTION



Coast Guard Station Restoration

Participants

This study would not have been possible were it not for the concern, dedication and vision of the City of Cleveland to save the Coast Guard Station, the commitment and perseverance of the planning team, and the many organizations and residents that supported this initiative and devoted their time and energy. We would like to thank the following for their active roles in the process:

Bob Brown – Planning Director – City of Cleveland

Debbie Berry – Lakefront Plan Manager – City of Cleveland

N. Kurt Wiebusch – Commissioner of Architecture – City of Cleveland

Paul Burik – Chief Architect of Dept. of Architecture – City of Cleveland

Matt Zone – City Council Ward 17 – City of Cleveland

Karla Kaulfuss – Landmarks Commission – City of Cleveland

George Cantor – Chief City Planner – City of Cleveland

Tom Nagel – Commissioner of Property Management – City of Cleveland

Paul Alsenas – Director – Cuyahoga County Planning Commission

Carol Thaler – Program Officer – Cuyahoga County Planning Commission

Kathleen Crowther – Executive Director – Cleveland Restoration Society

Tim Donovan - Director – Ohio & Erie Canalway

Jim Kastelic – Senior Park Planner – Cleveland Metroparks

John Debo - Superintendent – National Park Service

Pat Conway – Great Lakes Brewing Company

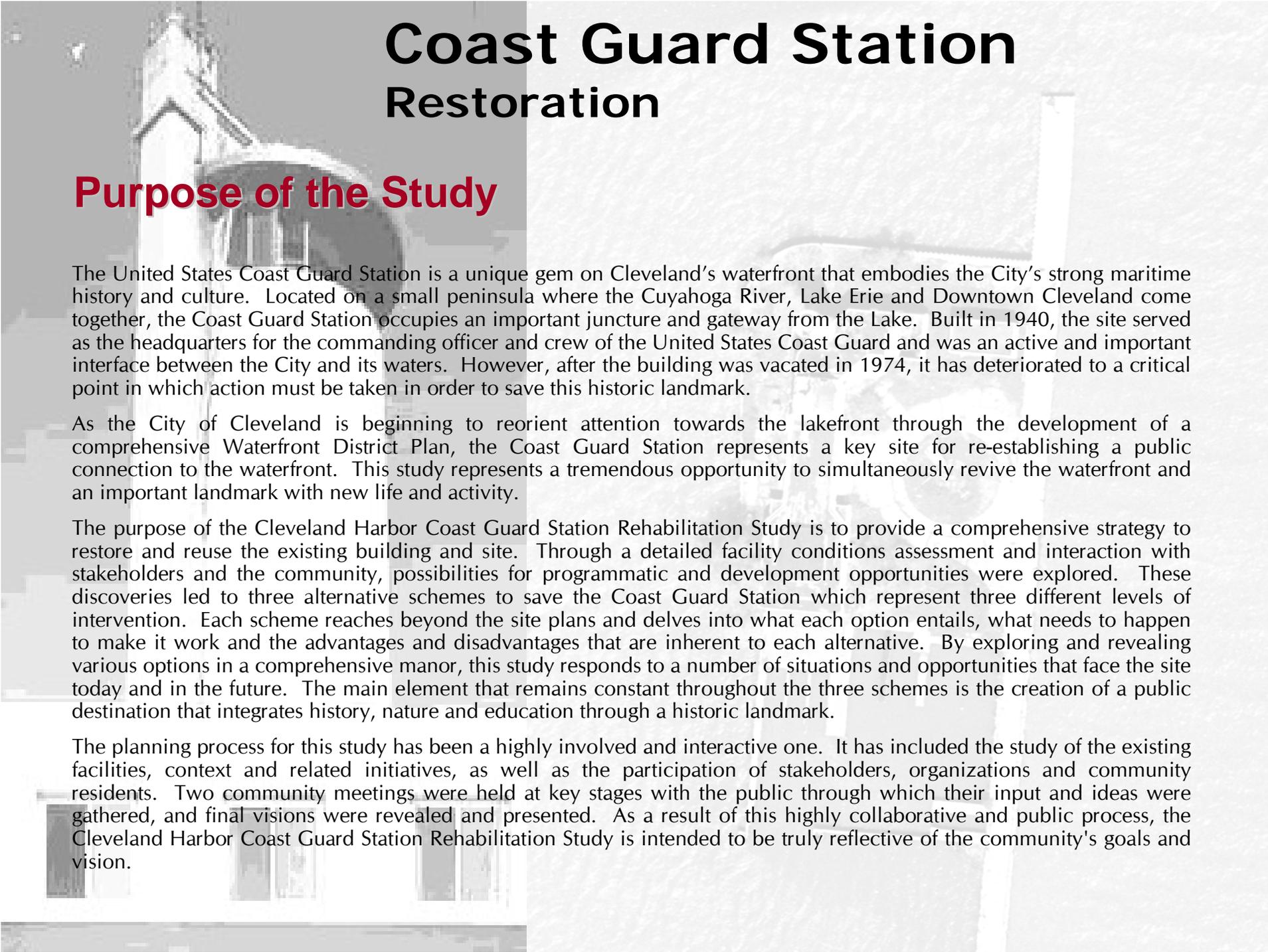
The attendees of the community meetings who participated in the development of this project.

Planning Report Produced by City Architecture

- Paul Volpe
- Greg Yanito
- Courtney Lepene

Engineering and Conditions Assessment by Ralph Tyler Companies

- Steven Birch
- Geoff Varga
- Andrew Kyprianou
- Melvin Holliman
- Hardip Singh
- Al Hughes



Coast Guard Station Restoration

Purpose of the Study

The United States Coast Guard Station is a unique gem on Cleveland's waterfront that embodies the City's strong maritime history and culture. Located on a small peninsula where the Cuyahoga River, Lake Erie and Downtown Cleveland come together, the Coast Guard Station occupies an important juncture and gateway from the Lake. Built in 1940, the site served as the headquarters for the commanding officer and crew of the United States Coast Guard and was an active and important interface between the City and its waters. However, after the building was vacated in 1974, it has deteriorated to a critical point in which action must be taken in order to save this historic landmark.

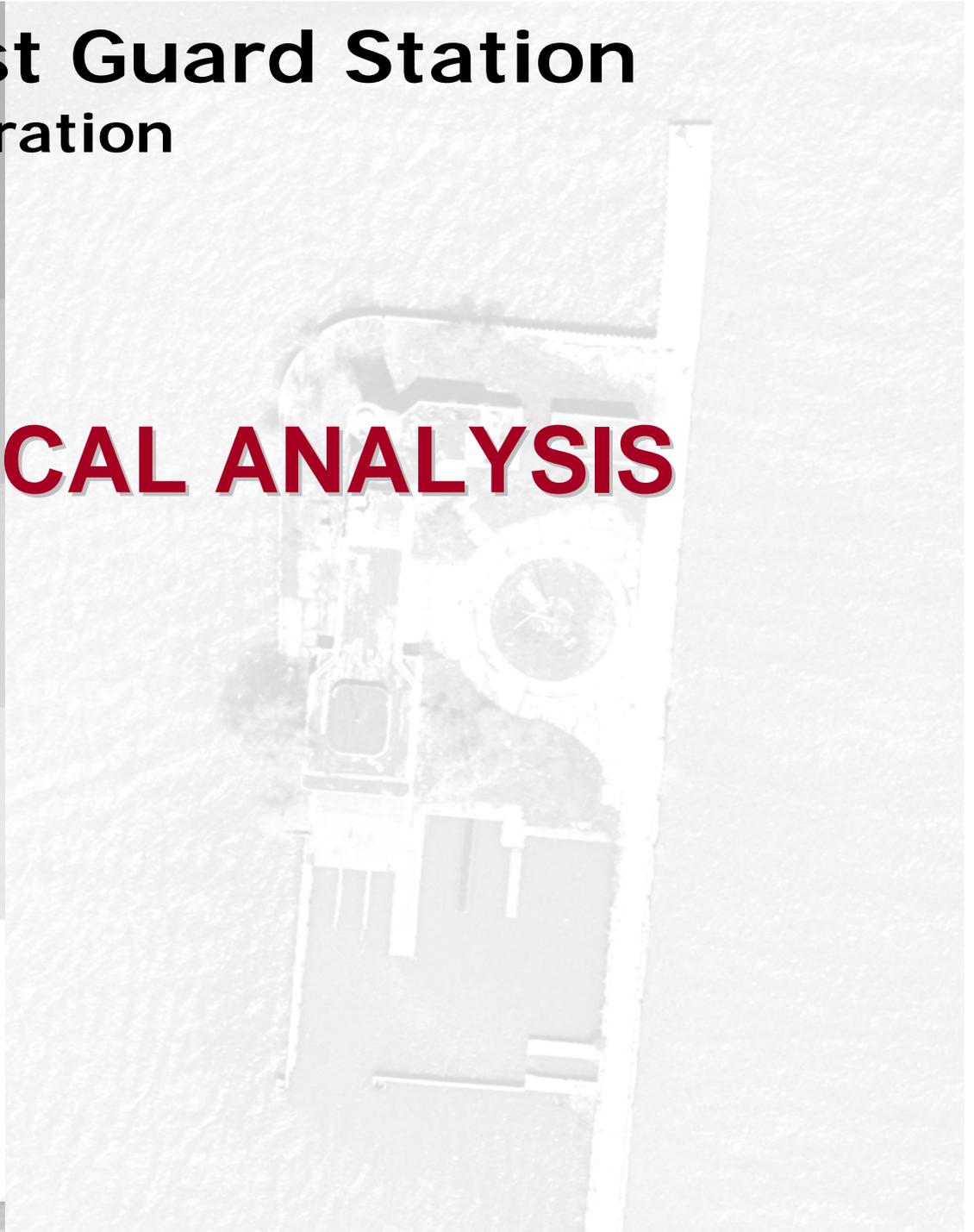
As the City of Cleveland is beginning to reorient attention towards the lakefront through the development of a comprehensive Waterfront District Plan, the Coast Guard Station represents a key site for re-establishing a public connection to the waterfront. This study represents a tremendous opportunity to simultaneously revive the waterfront and an important landmark with new life and activity.

The purpose of the Cleveland Harbor Coast Guard Station Rehabilitation Study is to provide a comprehensive strategy to restore and reuse the existing building and site. Through a detailed facility conditions assessment and interaction with stakeholders and the community, possibilities for programmatic and development opportunities were explored. These discoveries led to three alternative schemes to save the Coast Guard Station which represent three different levels of intervention. Each scheme reaches beyond the site plans and delves into what each option entails, what needs to happen to make it work and the advantages and disadvantages that are inherent to each alternative. By exploring and revealing various options in a comprehensive manor, this study responds to a number of situations and opportunities that face the site today and in the future. The main element that remains constant throughout the three schemes is the creation of a public destination that integrates history, nature and education through a historic landmark.

The planning process for this study has been a highly involved and interactive one. It has included the study of the existing facilities, context and related initiatives, as well as the participation of stakeholders, organizations and community residents. Two community meetings were held at key stages with the public through which their input and ideas were gathered, and final visions were revealed and presented. As a result of this highly collaborative and public process, the Cleveland Harbor Coast Guard Station Rehabilitation Study is intended to be truly reflective of the community's goals and vision.

Coast Guard Station Restoration

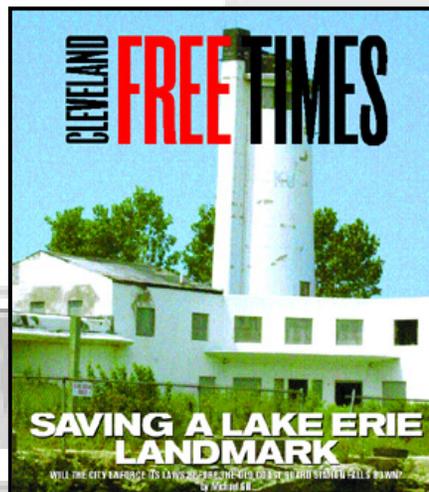
HISTORICAL ANALYSIS



Coast Guard Station Restoration

Why Preserve the Coast Guard Station?

- Cleveland Landmark
- Listed on National Register of Historic Places
- Architecture
- History
- Location, location, location – prominence as a visual landmark
- Views from the station
- Opportunities for Reuse/Rehabilitation



Coast Guard Station Restoration

History of the Site and Building



Lighthouse, 1885

Located at W. 9th Street/Lakeside



Life Saving Station, 1933 (built in 1897)

Throughout the late 1800's, Cleveland's coastline was scattered with numerous Life Saving Stations to help people in distress on the water and monitor navigational hazards at the entrances to harbors. In 1897, a station was built at the end of the west pier entrance of the Cuyahoga River on filled land. It consisted of four separate wood structures in the "Port Huron Style" and contained a main operations building, a boat house, equipment building and a lighthouse service building. As commerce and motor powered boats began to take over the waters, the duties of the Life Saving Service became more active and involved. To respond to the changing nature of the waterways, Congress passed a law in 1915 to form the United States Coast Guard, which prompted the consolidation of the many Life Saving Stations into new buildings that could accommodate the new structure and duties of the Coast Guard.

Coast Guard Station Restoration



MAIN BUILDING

Communications room, officer's quarters, mess hall, recreation room, store rooms, and on the second floor, crew and staff quarters + 60 foot lookout tower

GARAGE



BOAT HOUSE

three slips, maintenance space, and on the second floor, a tackle and work shop



In 1940, a new Coast Guard Station was built at the end of the west pier for a total cost of \$360,000. Built in the streamline moderne architectural style, the design was meant to resemble a lake vessel and recreate the experience of being shipboard. The design consisted of flat roofs and curved surfaces derived from those of a ship, a tower similar to a mast and small connected passageways throughout the building that conveyed the labyrinth configurations below the decks of a boat. Three distinct buildings made up the station, including a Quarters Building and Boat House connected by a portecochere and a separate garage. The commanding office and crew of the Coast Guard remained headquartered at this site for over 30 years where they protected the waters by rescuing the shipwrecked and those endangered by storm or flood, carrying food and supplies to isolated communities, and eliminating navigational hazards.

Coast Guard Station Restoration

Coast Guard Station Historic Photographs

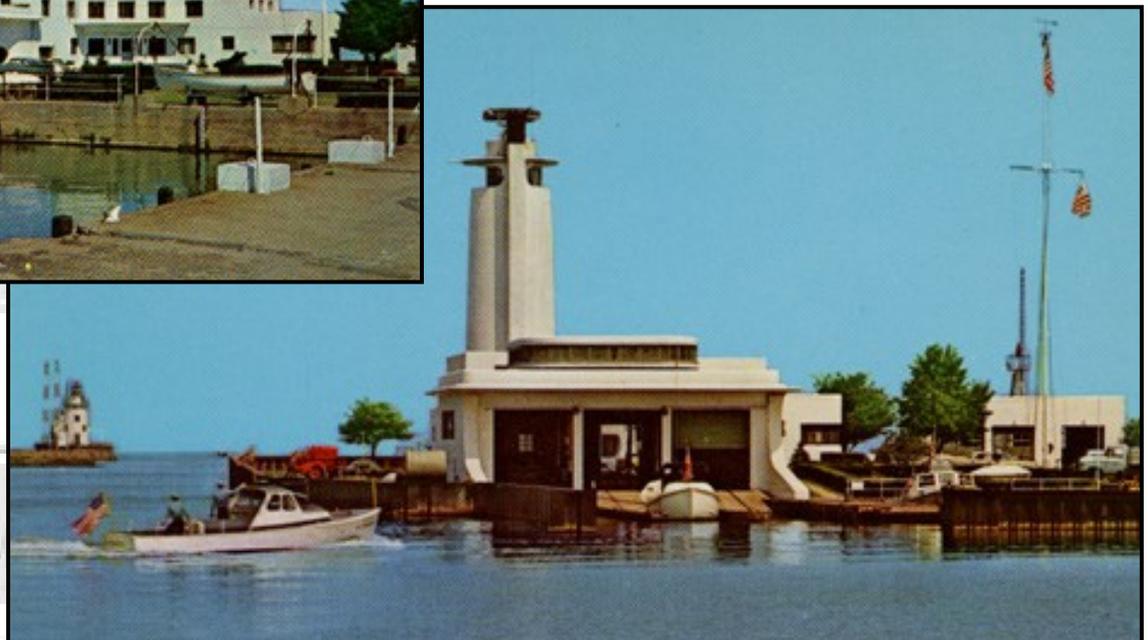


Described as “the finest” and “most beautiful in the nation,” the former Coast Guard Station is a relative rarity that is both functional and a work of art. It remains a nostalgic beacon of the past because of its uniqueness and its central role in Cleveland’s rich maritime history.



Coast Guard Station Restoration

Historic Postcards

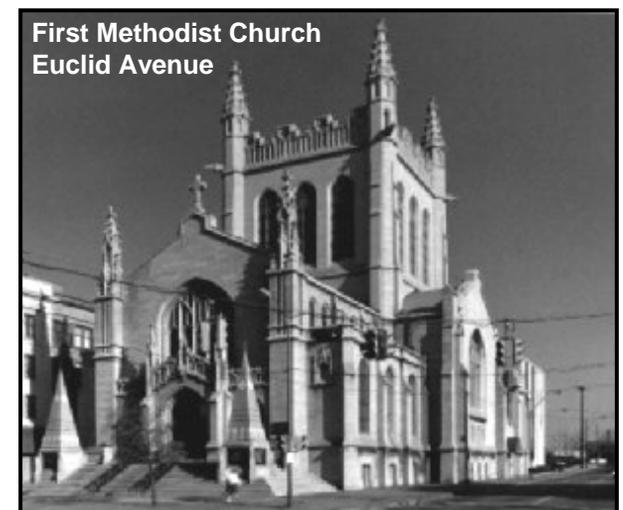


Because the waterfront and Coast Guard Station were so important to Cleveland's economy and history, they became the subject of numerous postcards, capturing the Coast Guard Station as a part of vintage Cleveland.

Coast Guard Station Restoration

Architect – J. Milton Dyer (1870 – 1957)

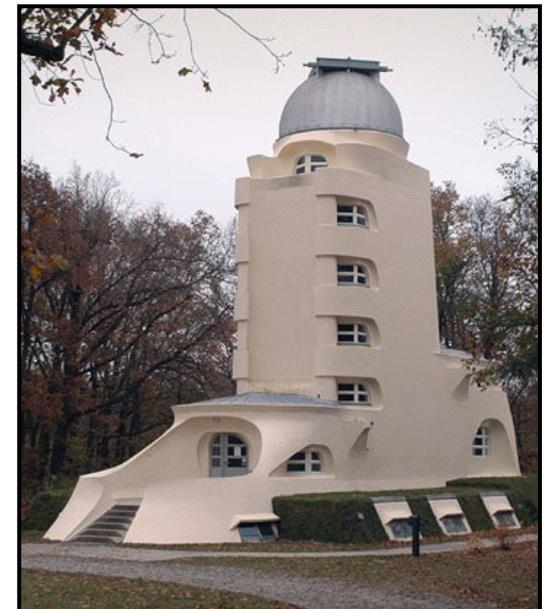
- Son of Pennsylvania hardware businessman.
- Attended Central High School, Case Institute, Ecole des Beaux Arts.
- 1906 Architectural Record published article on his work, highlighted as a professional architect living in a smaller city.
- His office staff included Walker & Weeks.
- Clevelander until his death. Never married.
- Known for his eclectic style.



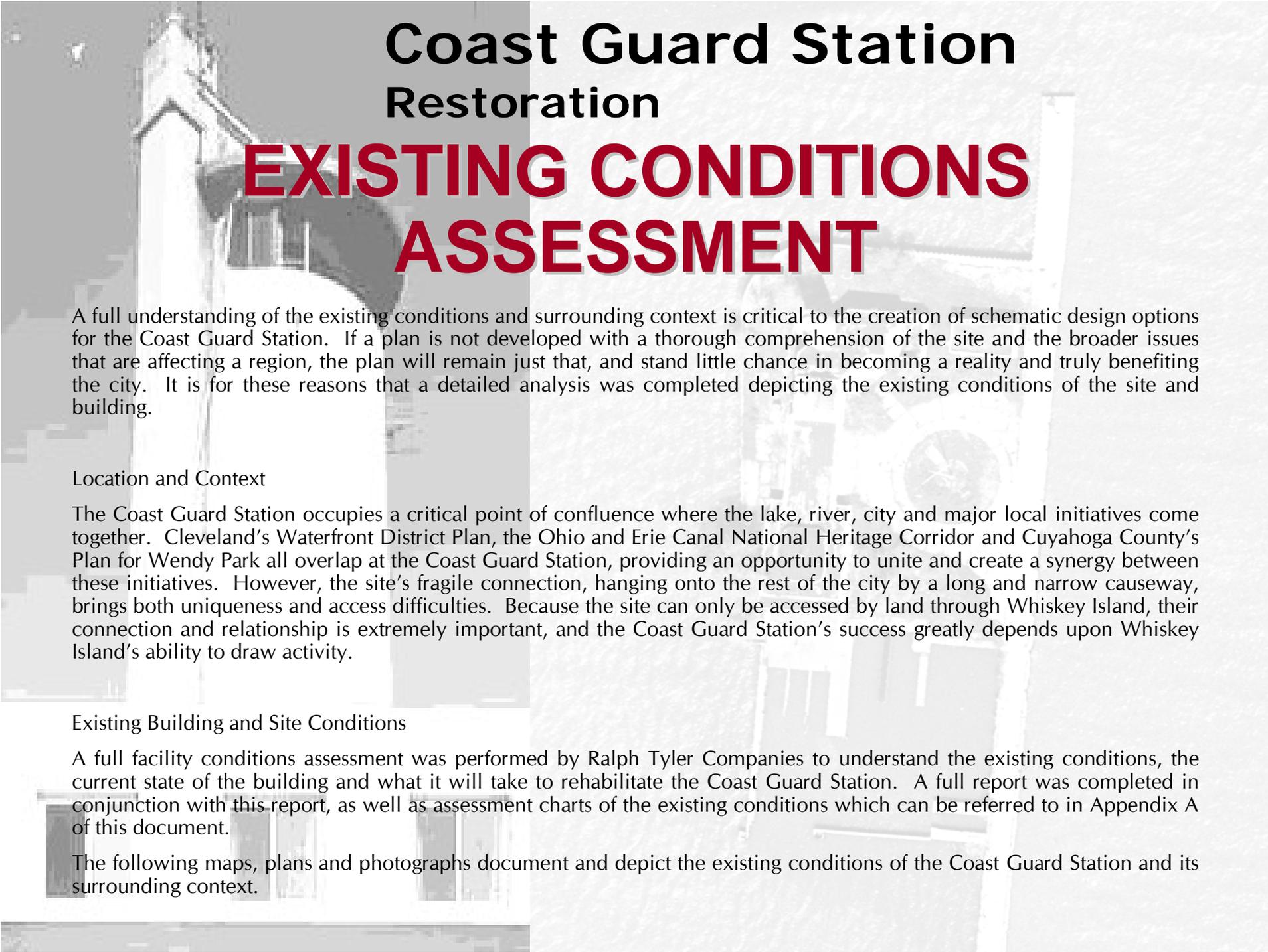
Coast Guard Station Restoration

Style – Streamline Moderne

	Moderne
Emphasis	Horizontal, Flat Roofs
Windows	Round, Glass Block, Horizontal Bands
Ornamentation	Smooth wall finish, cement, metal panels, little decorative
Features	Distinctive look with curved walls/windows, ship-like forms/details, asymmetrical, sleekness through sleek materials



Erich Mendelsohn's Einstein Tower, Potsdam, Germany
Shown at the Great Lakes Exposition in Cleveland



Coast Guard Station Restoration

EXISTING CONDITIONS ASSESSMENT

A full understanding of the existing conditions and surrounding context is critical to the creation of schematic design options for the Coast Guard Station. If a plan is not developed with a thorough comprehension of the site and the broader issues that are affecting a region, the plan will remain just that, and stand little chance in becoming a reality and truly benefiting the city. It is for these reasons that a detailed analysis was completed depicting the existing conditions of the site and building.

Location and Context

The Coast Guard Station occupies a critical point of confluence where the lake, river, city and major local initiatives come together. Cleveland's Waterfront District Plan, the Ohio and Erie Canal National Heritage Corridor and Cuyahoga County's Plan for Wendy Park all overlap at the Coast Guard Station, providing an opportunity to unite and create a synergy between these initiatives. However, the site's fragile connection, hanging onto the rest of the city by a long and narrow causeway, brings both uniqueness and access difficulties. Because the site can only be accessed by land through Whiskey Island, their connection and relationship is extremely important, and the Coast Guard Station's success greatly depends upon Whiskey Island's ability to draw activity.

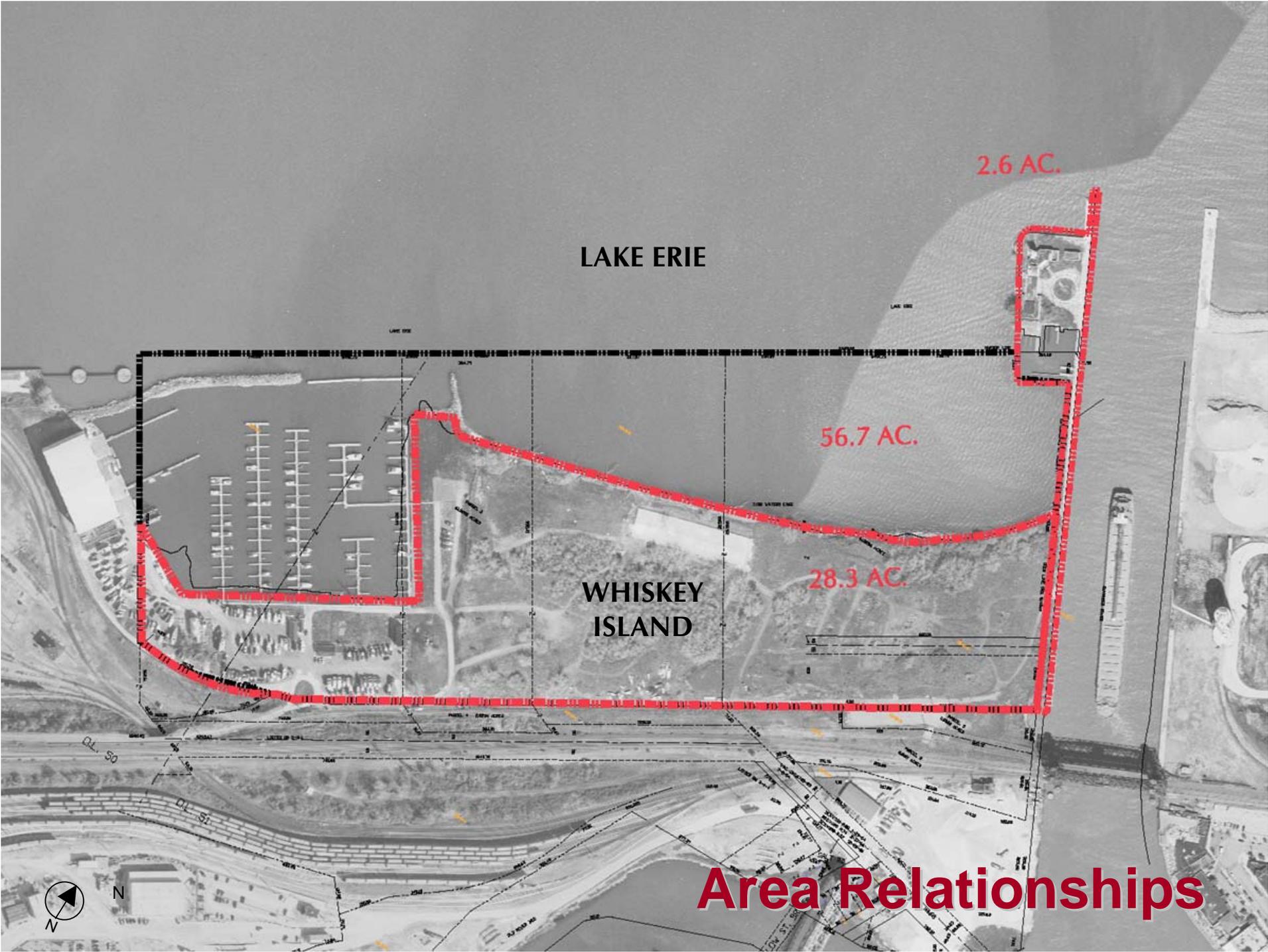
Existing Building and Site Conditions

A full facility conditions assessment was performed by Ralph Tyler Companies to understand the existing conditions, the current state of the building and what it will take to rehabilitate the Coast Guard Station. A full report was completed in conjunction with this report, as well as assessment charts of the existing conditions which can be referred to in Appendix A of this document.

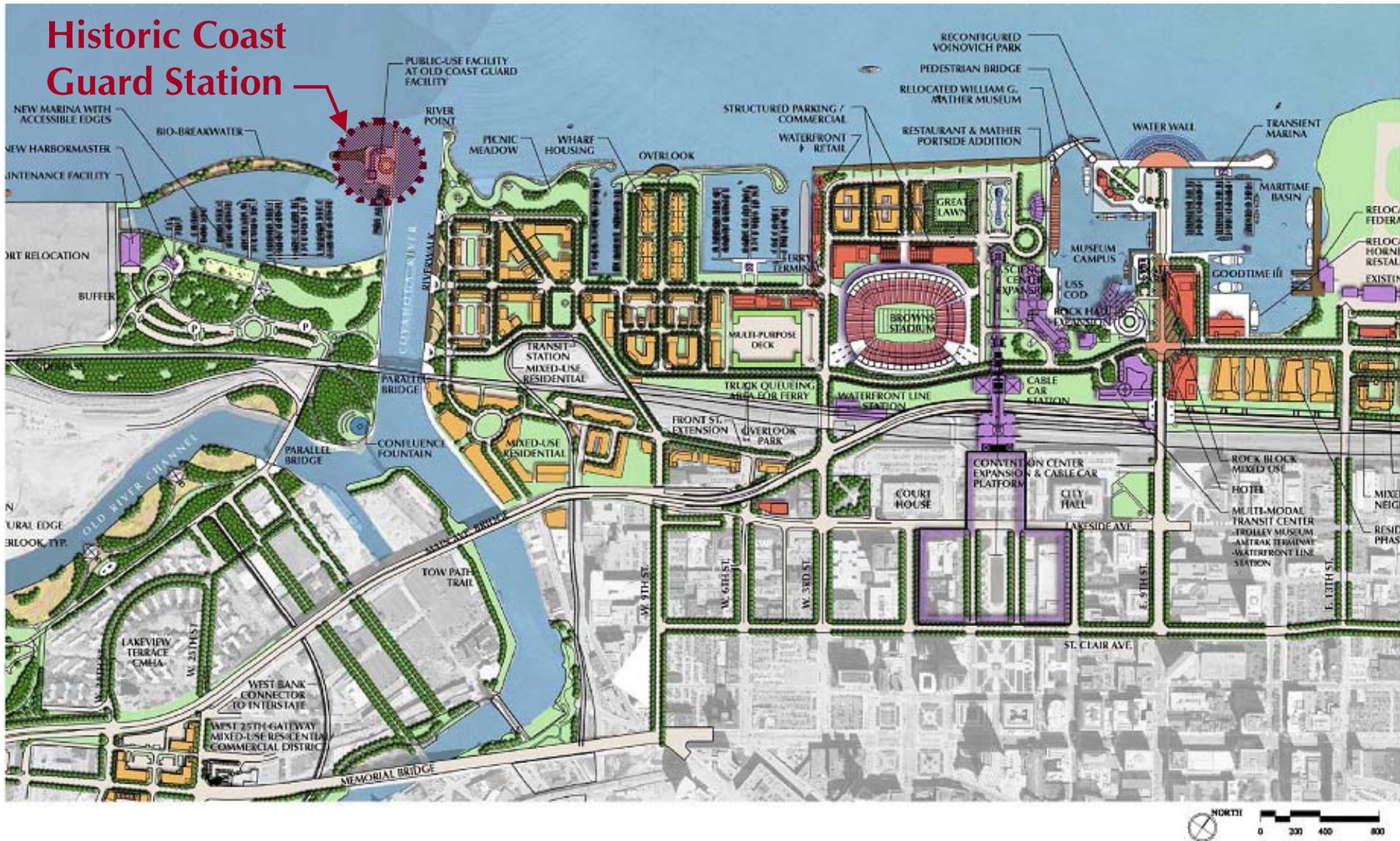
The following maps, plans and photographs document and depict the existing conditions of the Coast Guard Station and its surrounding context.



Contextual View



Historic Coast Guard Station



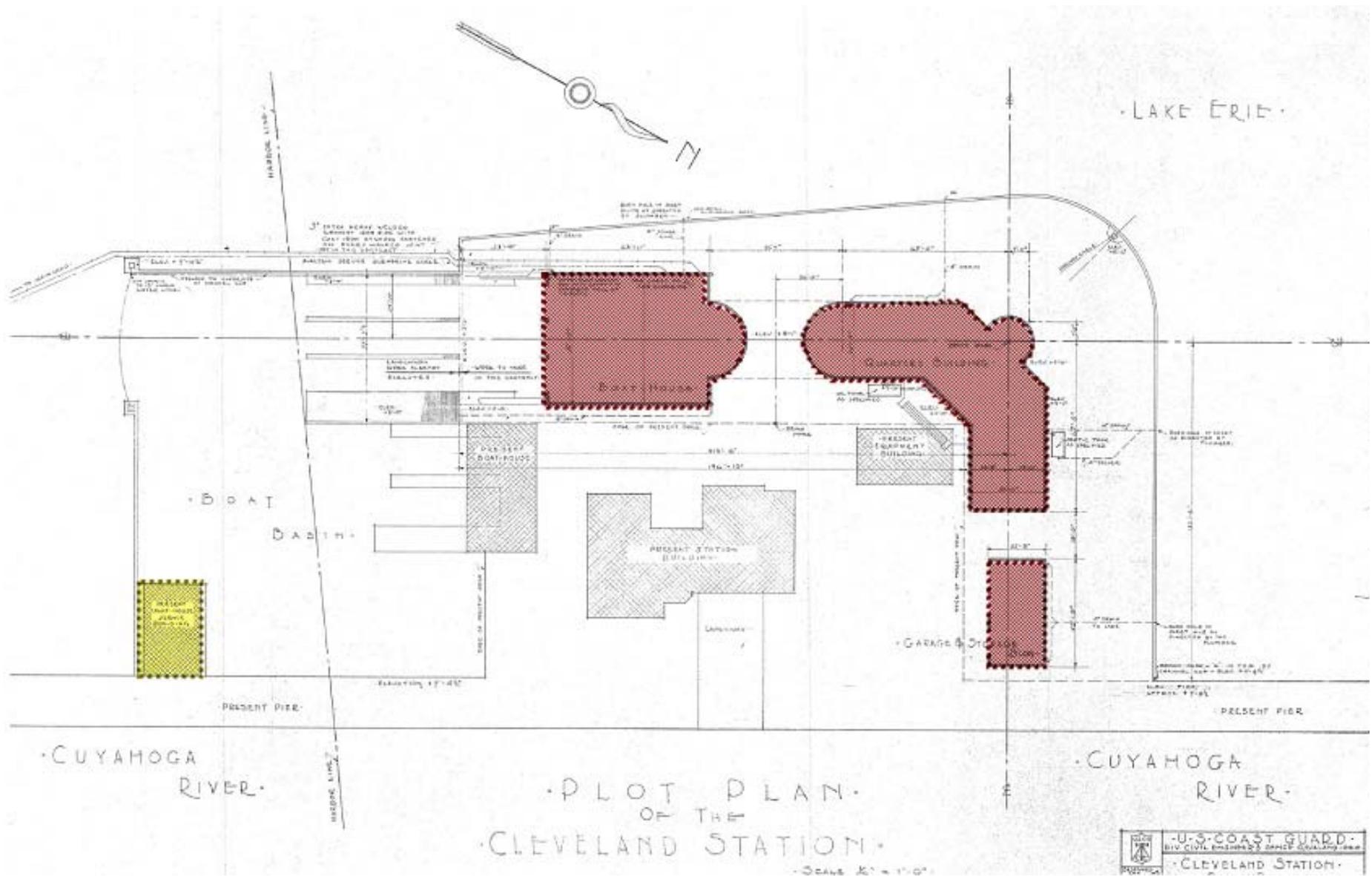
Cleveland Waterfront District Plan

**Coast Guard Site:
2.6 acres
including
causeway**

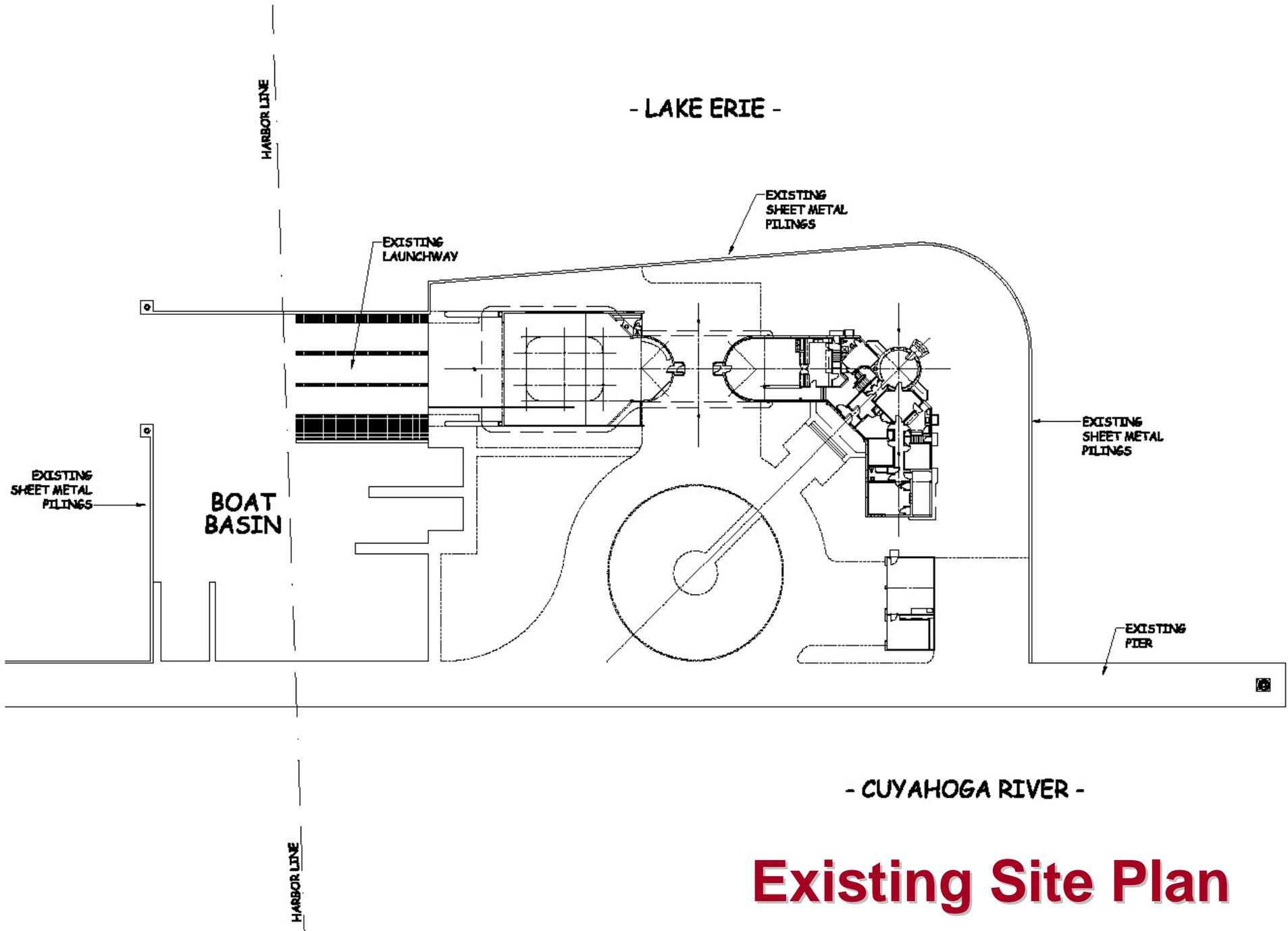


Aerial View

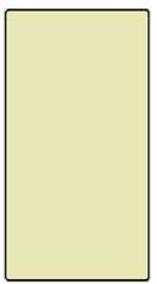
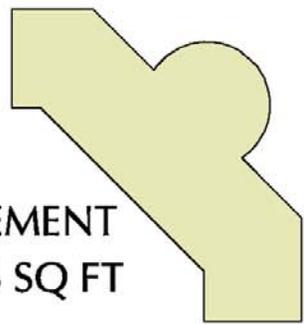




Historic Site Plan



Existing Site Plan

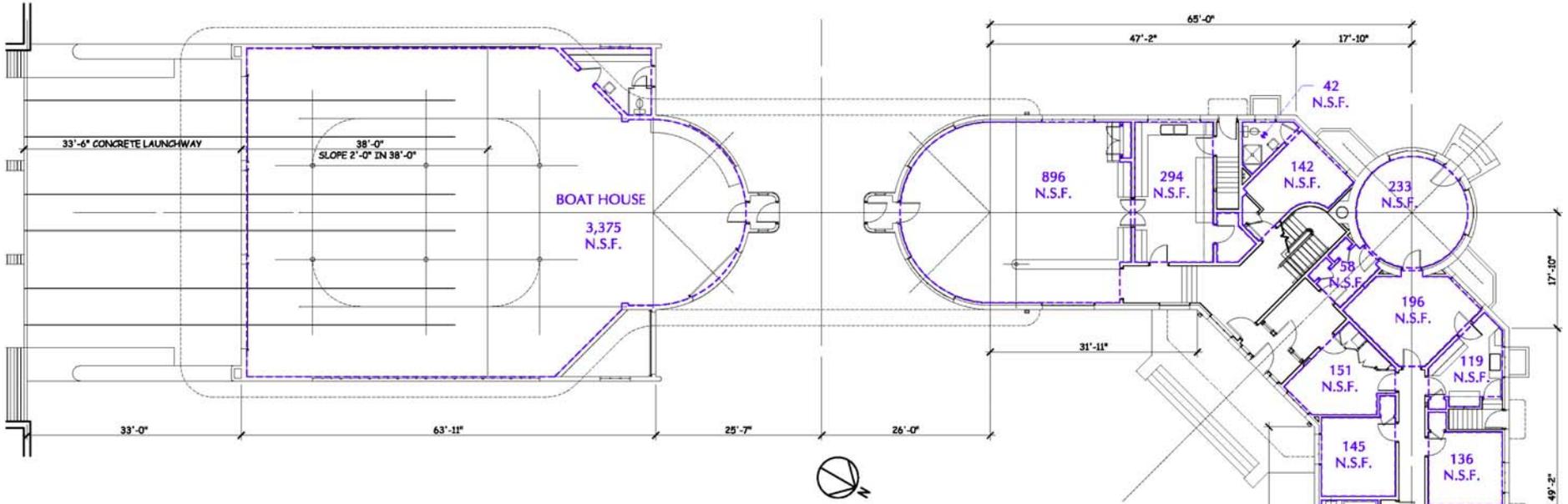


Gross Area:

Quarters Bldg 8,566 s.f.

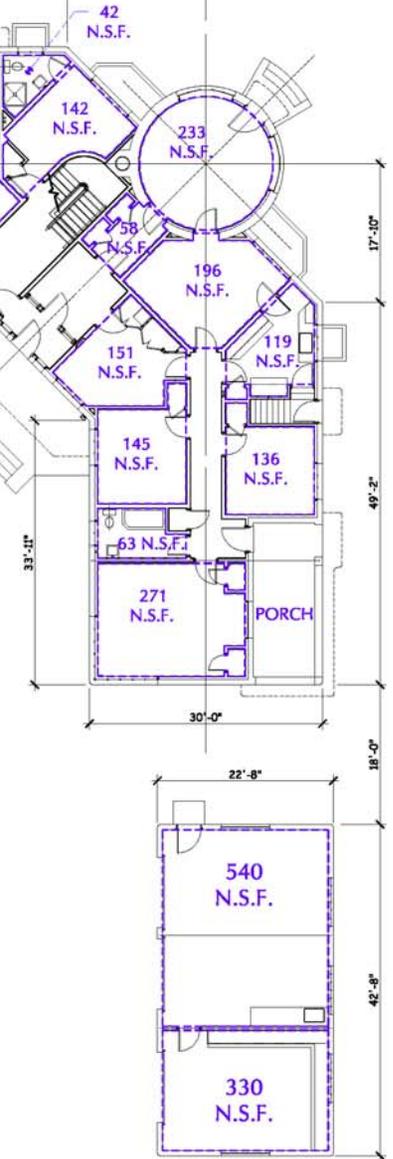
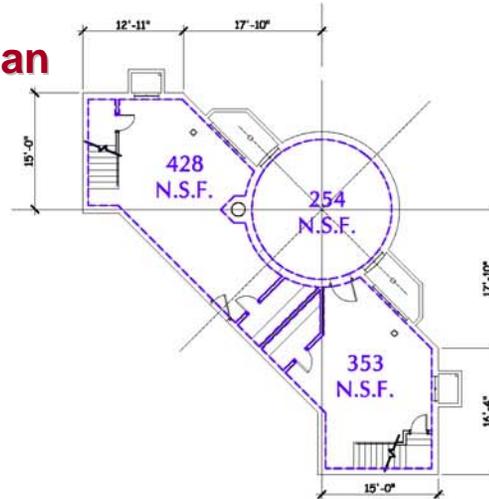
Total Campus 13,231 s.f.

Area Calculations



Basement Level

Floor Plan

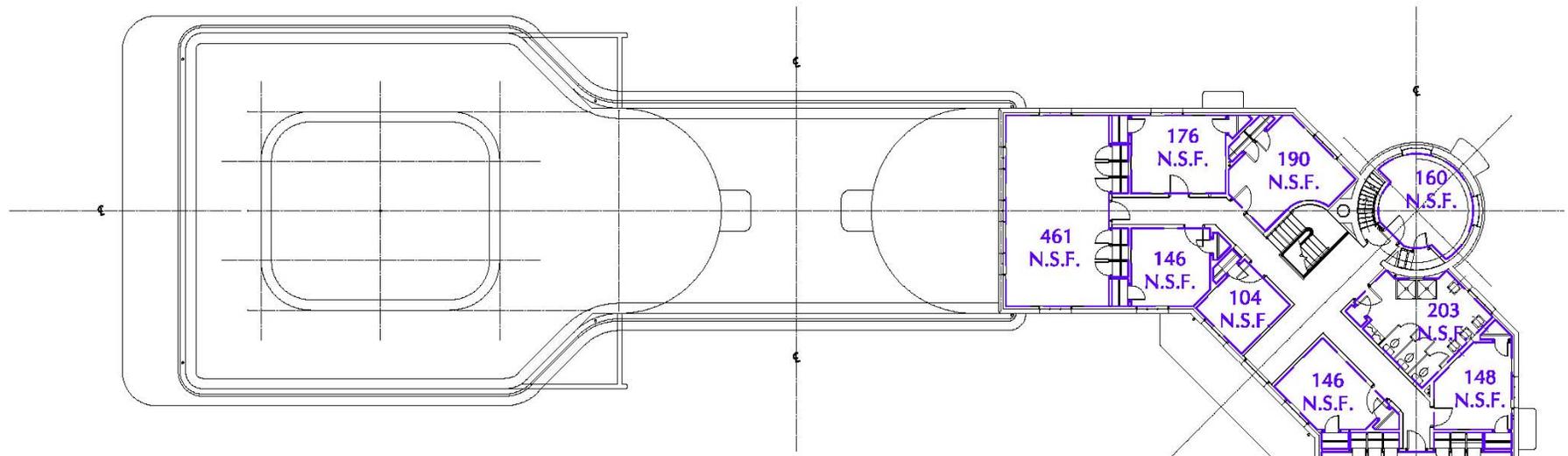


Net Area:

Boat House	3,375 s.f.
Quarters Bldg.	2,745 s.f.
Garage	1,035 s.f.
Basement	780 s.f.

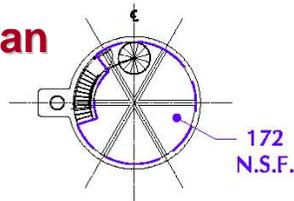
Ground Level Plan

*areas do not include circulation space



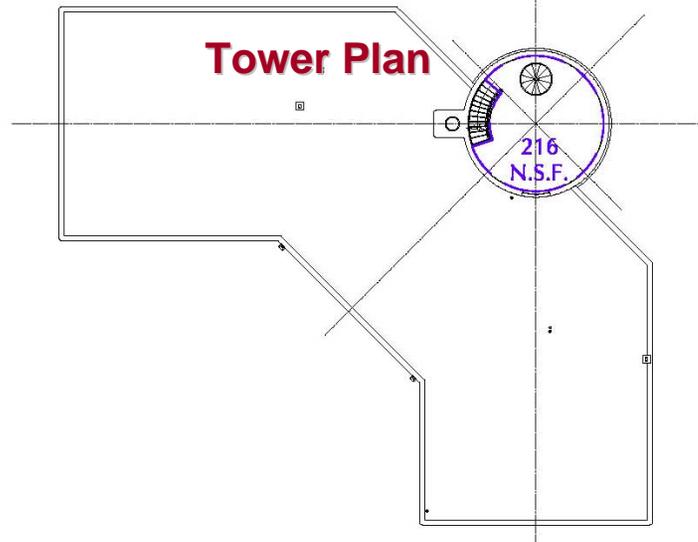
Fourth Level

Tower Plan



Third Level

Tower Plan



Quarters Bldg. 2,190 s.f.

Tower (3rd level) 215 s.f.

Tower (4th level) 170 s.f.

Tower (5th level) 75 s.f.

Tower (6th level) 90 s.f.

TOTAL BLDG: 10,675 s.f. +/-

*areas do not include circulation space

Second Level Plan

Coast Guard Station Restoration

Exteriors:

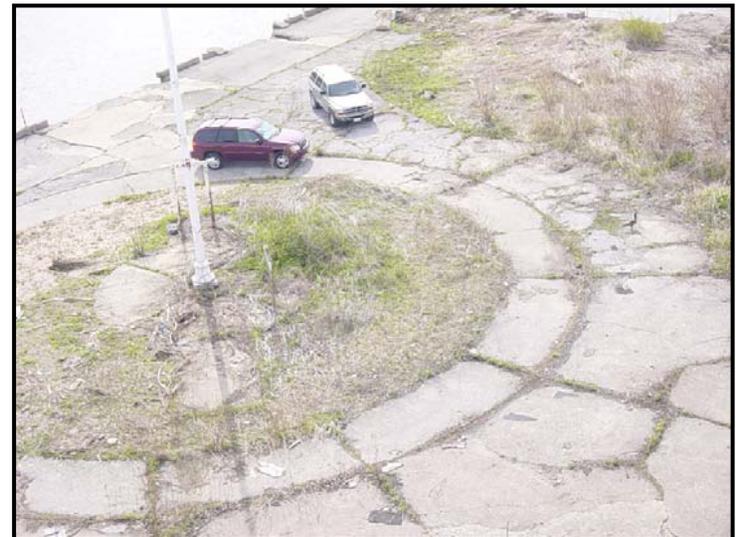
Roadway Assessment:

- 20 x 1000 foot pier access.
- Access impassible.
- Heavy deterioration to pier elements.



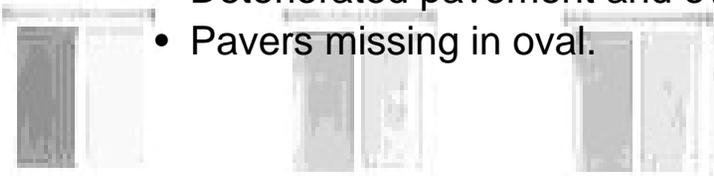
Pier must be rebuilt along
entire length.

Coast Guard Station Restoration



Site Assessment:

- 1.25 acre site after 1939.
- Deteriorated pavement and overgrowth.
- Pavers missing in oval.



Coast Guard Station Restoration

Site Utilities:

- Most utilities missing or unverifiable.
- Main building drained to septic tank.
- Sanitary from Boathouse & Garage drain directly to Lake. Some storm combined.
- Gas service unverifiable.
- Above ground water line subject to freezing in winter.
- Underwater electrical service cable.



Coast Guard Station Restoration

Exterior Assessment:

- Wall structure good condition.
- Exterior skin poor condition.
- Moisture intrusion behind painted surfaces.
- Doors and windows are missing.
- Wood canopies are failing.
- Downspouts and flashings missing.



Coast Guard Station Restoration

Roofs:

- Existing flat built-up roofs with copper flashings.
- Connector roof wood deck with canvas roofing.
- All roofs have failed and have partially collapsed except Garage.



Coast Guard Station Restoration

Exterior Accessibility:

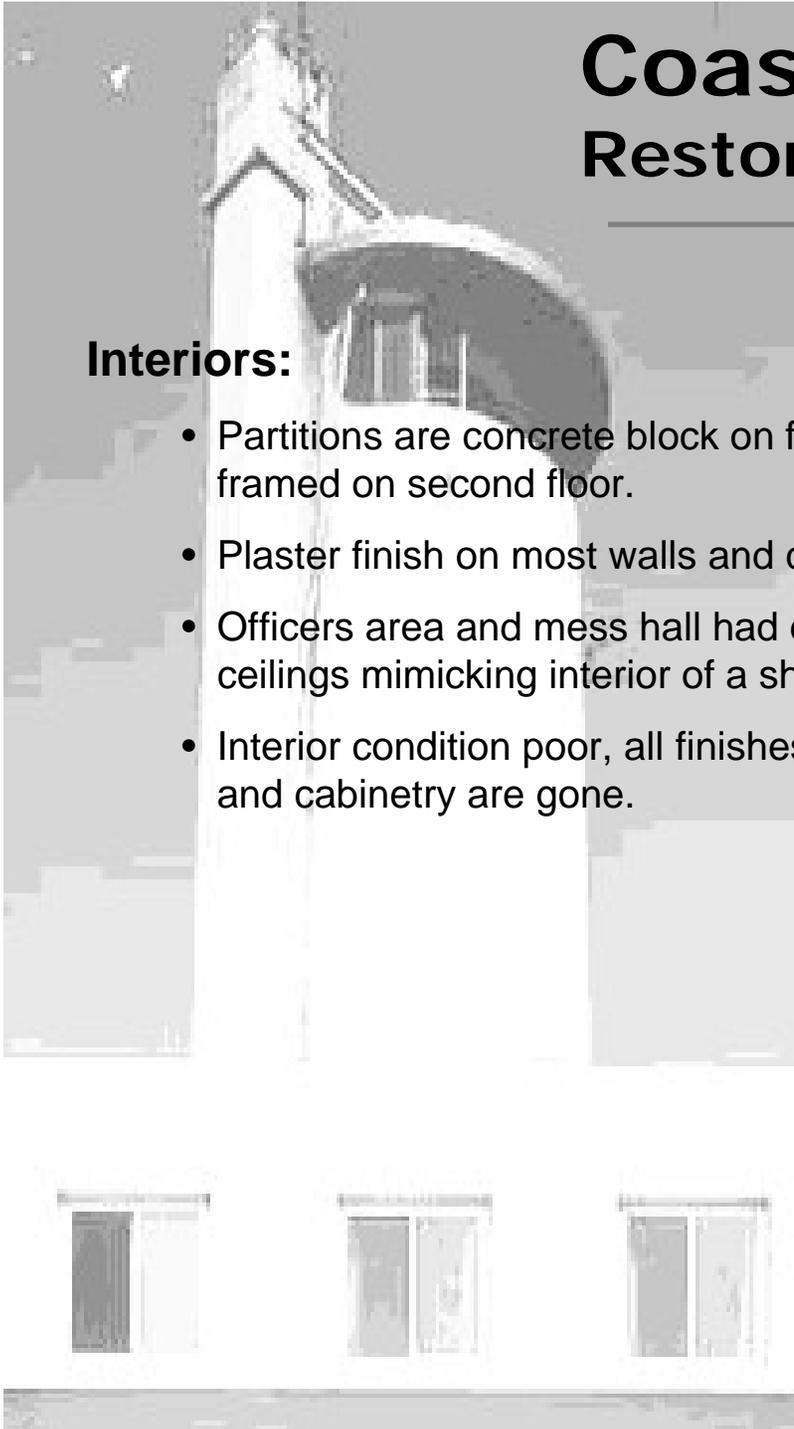
- Site will accommodate handicapped accessible parking spaces and handicapped accessible access.
- Ramps objectionable to historic character.
- Accessibility feasible without an exterior ramp.



Coast Guard Station Restoration

Interiors:

- Partitions are concrete block on first floor, wood framed on second floor.
- Plaster finish on most walls and ceilings.
- Officers area and mess hall had decorative ceilings mimicking interior of a ship.
- Interior condition poor, all finishes, trim, partitions and cabinetry are gone.



Coast Guard Station Restoration

Interiors:

- Open roof / water intrusion accelerates deterioration of structural elements.
- Basement flooded with water.
- Original plan does not meet ADA. Stairs not code compliant.



Coast Guard Station Restoration

Structural:

•Boathouse:

- Exterior load bearing reinforced concrete walls are good with no evidence of spalling or exposed reinforcement.
- Large cracks in the arched retaining walls located east and west of the boat ramp apron.
- Boat docks are deteriorated and need to be rebuilt.



Coast Guard Station Restoration



Structural:

•Quarters Building:

- Structural steel members in the main building have been rust-proofed and they do not exhibit any indications of section loss or corrosion.
- Steel frame of the monitor constructed above the main building exhibits signs of surface corrosion due to weather exposure, however no apparent section loss .
- Entire wood joist and wood deck roof construction has completely deteriorated.

Coast Guard Station Restoration

- Original flat roofs wood decking over wood joist construction. Eventually replaced with a wood truss gable roof.



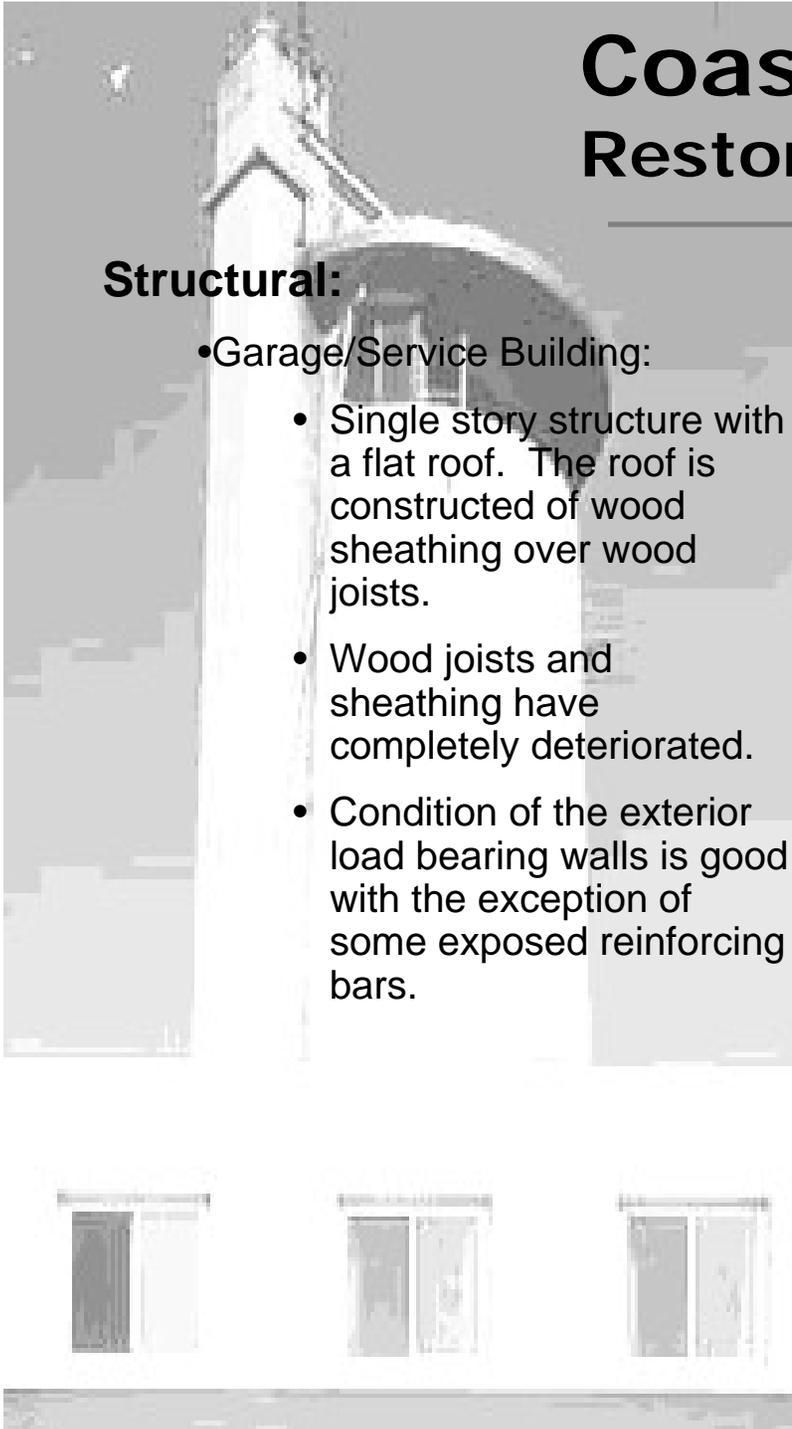
Second floor wood joist and wood sheathing has completely deteriorated.

Coast Guard Station Restoration

Structural:

•Garage/Service Building:

- Single story structure with a flat roof. The roof is constructed of wood sheathing over wood joists.
- Wood joists and sheathing have completely deteriorated.
- Condition of the exterior load bearing walls is good with the exception of some exposed reinforcing bars.



Coast Guard Station Restoration

Structural:

•Conclusions/Recommendations:

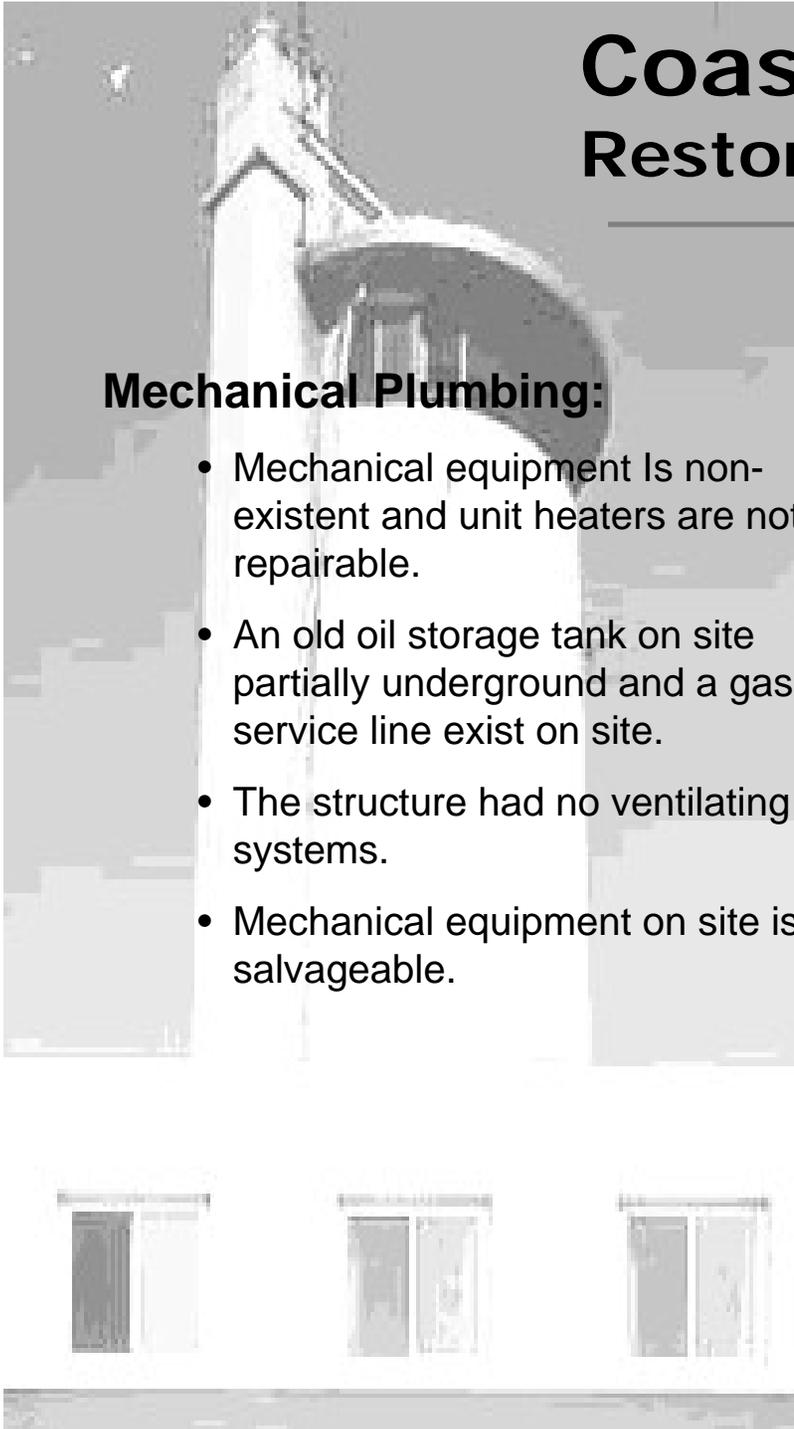
- The three buildings are structurally sound with the exception of the timber members and some minor concrete and reinforcement repair.
- Wood joists and wood decking will have to be removed and replaced in their entirety.
- Spalled concrete and exposed reinforcing shall be repaired by removing any surface rust from the reinforcing and subsequently repairing the concrete.



Coast Guard Station Restoration

Mechanical Plumbing:

- Mechanical equipment is non-existent and unit heaters are not repairable.
- An old oil storage tank on site partially underground and a gas service line exist on site.
- The structure had no ventilating systems.
- Mechanical equipment on site is not salvageable.



Coast Guard Station Restoration



Electrical System:

- Incoming service was routed exposed running along the pier to a pole mounted transformer at 480 volt which was stepped down to 120/240 volt via transformer located in the boathouse.
- Branch panels exist, but are in very poor condition and not suitable for reuse.
- Building wiring must be brought up to code.



Coast Guard Station Restoration



Electrical:

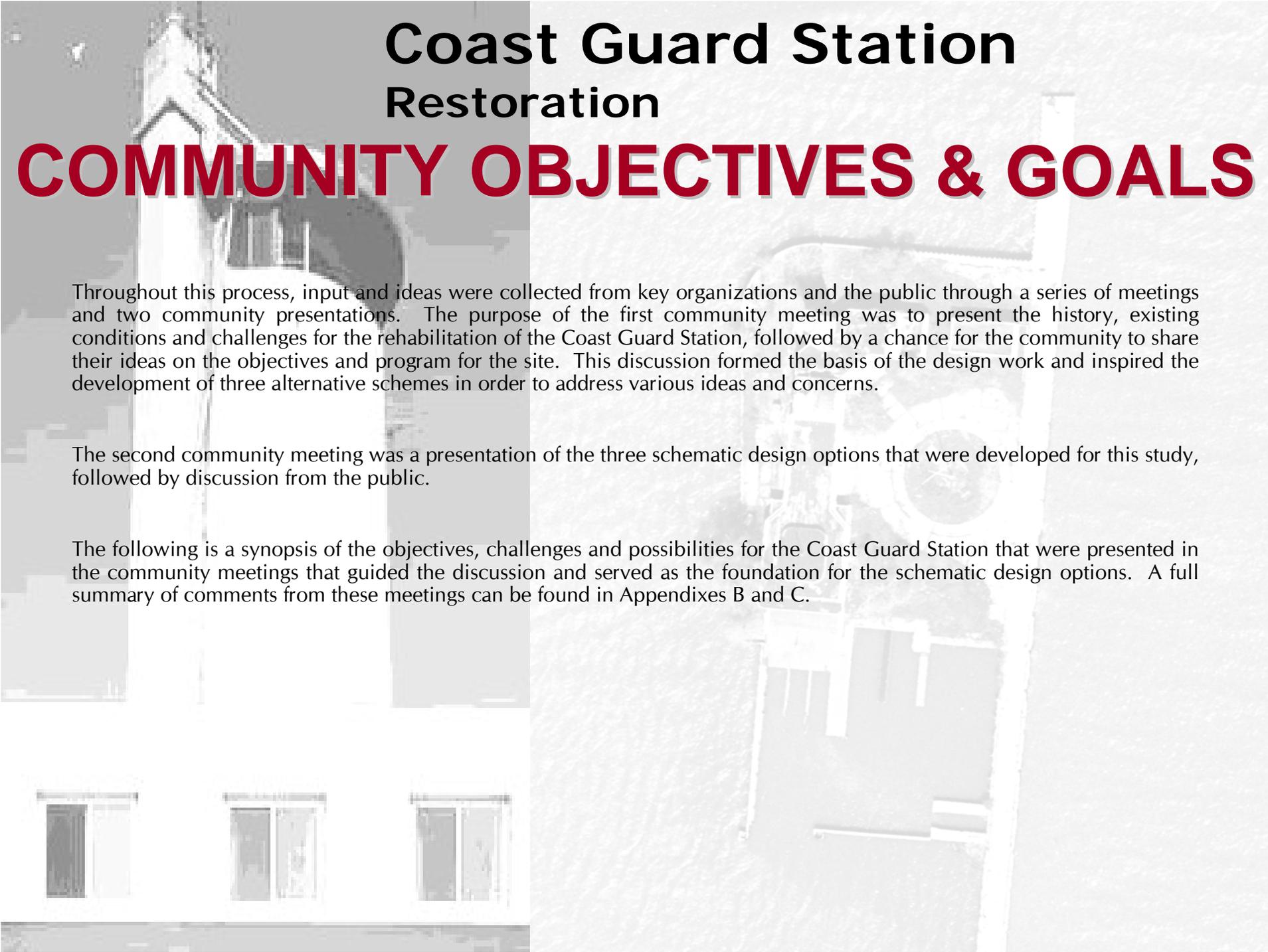
- Lighting:
 - Original fixtures all incandescent.
 - Fluorescent fixtures added later.
 - Lighting system is in very poor condition.
- Emergency/egress lighting:
 - Non-existent except boathouse.
- Security systems: non-existent.
- Exterior building mounted and site lighting: non-existent.
- Fire alarm: non-existent.
- Life safety: non-existent.

Coast Guard Station Restoration

Summary:

- The buildings' reinforced concrete walls should be saved and renovated. Existing structural should be cleaned and recoated.
- Wood decking and wood framing should be replaced with steel members.
- All other building systems must be gutted, replaced and brought up to code.





Coast Guard Station Restoration

COMMUNITY OBJECTIVES & GOALS

Throughout this process, input and ideas were collected from key organizations and the public through a series of meetings and two community presentations. The purpose of the first community meeting was to present the history, existing conditions and challenges for the rehabilitation of the Coast Guard Station, followed by a chance for the community to share their ideas on the objectives and program for the site. This discussion formed the basis of the design work and inspired the development of three alternative schemes in order to address various ideas and concerns.

The second community meeting was a presentation of the three schematic design options that were developed for this study, followed by discussion from the public.

The following is a synopsis of the objectives, challenges and possibilities for the Coast Guard Station that were presented in the community meetings that guided the discussion and served as the foundation for the schematic design options. A full summary of comments from these meetings can be found in Appendixes B and C.

OBJECTIVES & GOALS

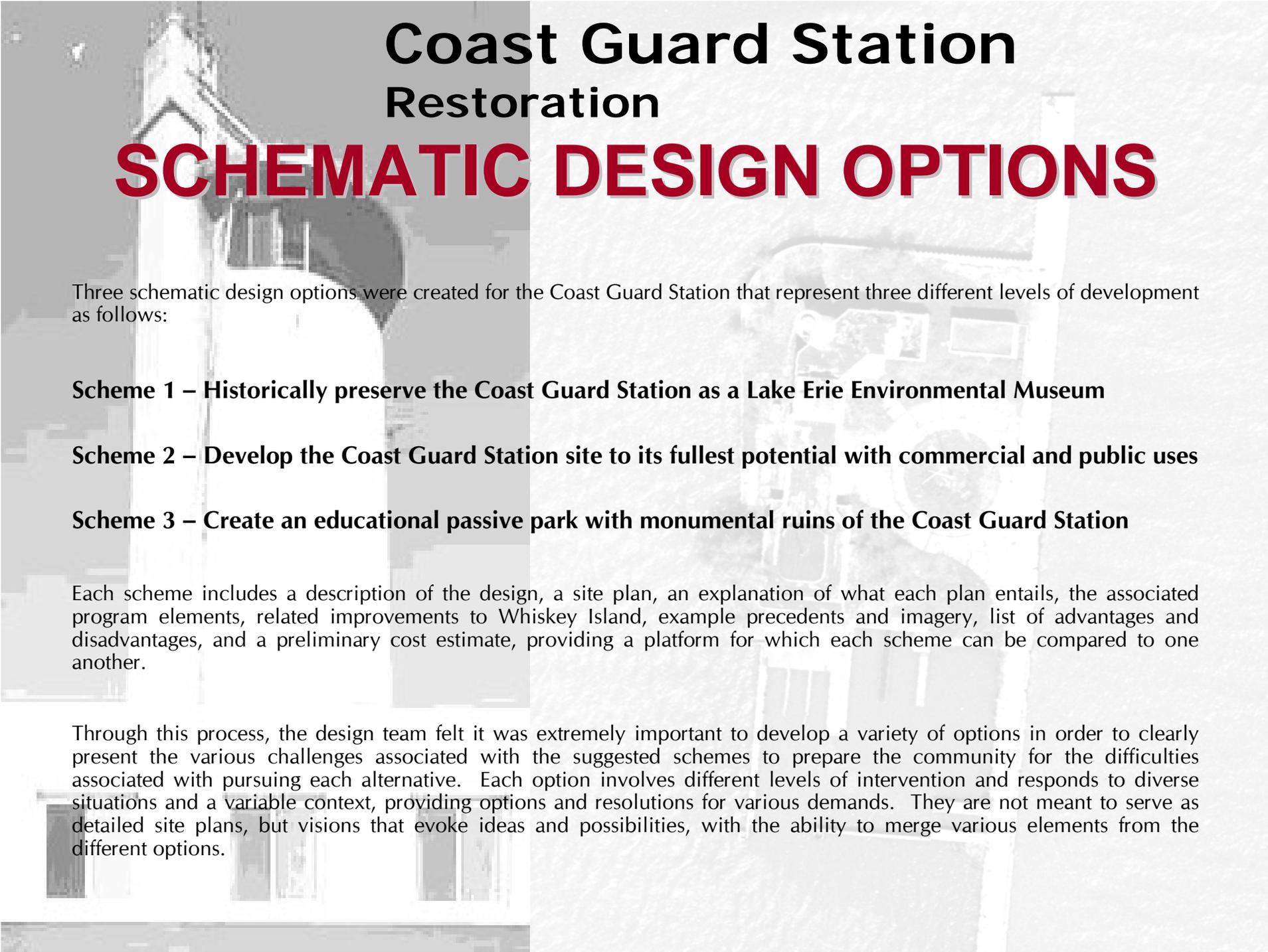
1. Save Historic Structure
2. Tell the History of the Station
3. Create a Destination & Gateway
4. Program Viable Year-Round Uses
5. Make Accessible by Multiple Transit Modes:
 - Car
 - Boat
 - Walking
6. Tie the Station into the Waterfront District, Ohio & Erie Canalway and Whiskey Island Park Plan
7. Create an Economically, Sustainable Investment

CONSTRAINTS & CHALLENGES

1. Significant Restoration Cost
2. Lack of Utilities
3. Poor Accessibility
4. Need for Parking
5. Security Concerns
6. Operating and Maintenance Issues

POSSIBILITIES & POTENTIAL USES

1. Should we consider the site as an Educational or Interpretive Center?
 - History
 - Ecology / Nature
 - Environmental Laboratory
 - Maritime Museum
2. Should we consider Community Outreach Programs?
 - Youth Learning Classrooms
 - Boating / Water Safety
3. Should we consider Food-Related Services?
 - Visitor / Boater Concessions
 - Marina Restaurant
 - Full-Service Restaurant
4. Should we consider Recreational Uses?
 - Boating Club
 - Water-Sports Equipment Rentals
 - Park-Related Activities
5. Should we consider a Hospitality Use?
 - Bed & Breakfast
 - Youth Hostel
 - Transient Marina Lodging



Coast Guard Station Restoration

SCHEMATIC DESIGN OPTIONS

Three schematic design options were created for the Coast Guard Station that represent three different levels of development as follows:

Scheme 1 – Historically preserve the Coast Guard Station as a Lake Erie Environmental Museum

Scheme 2 – Develop the Coast Guard Station site to its fullest potential with commercial and public uses

Scheme 3 – Create an educational passive park with monumental ruins of the Coast Guard Station

Each scheme includes a description of the design, a site plan, an explanation of what each plan entails, the associated program elements, related improvements to Whiskey Island, example precedents and imagery, list of advantages and disadvantages, and a preliminary cost estimate, providing a platform for which each scheme can be compared to one another.

Through this process, the design team felt it was extremely important to develop a variety of options in order to clearly present the various challenges associated with the suggested schemes to prepare the community for the difficulties associated with pursuing each alternative. Each option involves different levels of intervention and responds to diverse situations and a variable context, providing options and resolutions for various demands. They are not meant to serve as detailed site plans, but visions that evoke ideas and possibilities, with the ability to merge various elements from the different options.

The Development Plan

This first option, suggested by community involvement, creates a historical and educational retreat by transforming the Old Coast Guard Station into a Lake Erie Environmental Museum. By preserving and restoring the buildings and site to reflect their original state, a historical showcase is created for people to discover, experience, reflect and enjoy.

The buildings and site that make up the 2.6 acres of the Coast Guard Station would work together to tell the story of the Great Lakes and Cuyahoga River through displays setup in existing rooms of the Coast Guard Station Quarters Building, through large-scale exhibits and shows on display in the original Boat House Building, to historical boats and maritime artifacts for visitors to view and interact with as they travel through this park-like enclave. Maritime and environmental education would also be portrayed throughout the exhibits, serving as a place for children, families and boaters to learn about issues that affect our water and contribute to the sustainability of our city.

With public boat docking around the entire site and causeway, the Coast Guard Station becomes a unique destination for boaters. A small restaurant / café within the original mess hall serves both museum visitors and boaters, bringing informal and everyday activity to the site.

Another unique draw that sets this site apart from any other is its panoramic views of Lake Erie and Downtown Cleveland. By providing public access to the original Observation Tower, visitors can experience not only incredible views, but the perspective of the Coast Guard crew that protected our waterways.

The remoteness of the site, located about ¼ mile or a 5-minute walk from Whiskey Island presents accessibility challenges, but it also adds to the Coast Guard Station's mystique and charm. The general public can travel to the site by water taxi or shuttle bus stationed at the Whiskey Island parking lot, as well as by boat, bike or walking, adding to the experience of visiting the Coast Guard Station.

SCHEME 1 HISTORICALLY PRESERVE THE COAST GUARD STATION AS A **LAKE ERIE ENVIRONMENTAL MUSEUM**

With the sole use of a museum and small café, the Coast Guard Station would be limited to daytime operations to ensure safety and security. In addition, because most access to the site is by boat, the site would only be open 9 months of the year, closing during the winter.

This option provides the opportunity to educate people on maritime history and environmental issues through the preservation of a historic landmark. Not only does it create a unique destination, but the Coast Guard Station is a key structure that supports and ties 3 major local initiatives together. The transformation of the Coast Guard Station into a Lake Erie Environmental Museum in a park-like setting relates to and complements the sustainable theme proposed for Wendy Park at Whiskey Island, promotes the Lakefront Plan's goal of providing greater access to the waterfront, and creates a park space that can become a part of the Ohio-Erie Canal National Heritage Corridor.



Historic Preservation of the site and buildings would give the public the chance to experience the Coast Guard Station in its original state.

SCHEME 1

HISTORICALLY PRESERVE THE COAST GUARD STATION AS A LAKE ERIE ENVIRONMENTAL MUSEUM

Key Program Elements

- Historically Preserved Building & Site
- Lake Erie Environmental Museum
- Observation Tower
- Park Space
- Small Restaurant / Café
- Public Boat Docking



SCHEME 1 HISTORICALLY PRESERVE THE COAST GUARD STATION AS A LAKE ERIE ENVIRONMENTAL MUSEUM

What would this entail?

Causeway / Roadway

- Rebuild road, curb and guardrail, widening the causeway from its current width of 20'-0" to 25'-0" to allow for 2-way limited vehicular access and pedestrian / bike traffic.
- Traffic on the causeway would be limited to shuttle buses, ADA accessibility, limited employee traffic, service and deliveries, emergency vehicles, bikes and pedestrian traffic.
- Install bollards along the west face of the causeway to allow public boat docking along the entire length.

Site

- Clear overgrown brush and deteriorated pavement.
- Replace pavement according to the original site plan, with additional walkways to enhance the park-like nature and public space environment, including a 10'-0" pedestrian promenade along the water and special paving at the main entrances of the buildings.
- Minimal landscaping is needed to reflect the original setting and create a park atmosphere.

Site Utilities

- Because all utilities are substantially missing, damaged or non-existent, all utilities should be rebuilt retrofitting green technology such as geothermal, solar power, wind power, a satellite generator program by Cleveland Public Power, etc. so that the buildings and systems can be 100% sustainable and "off the grid" as if the site were a true island and independent from surrounding utilities, serving as a sustainable showcase to educate visitors.

SCHEME 1 HISTORICALLY PRESERVE THE COAST GUARD STATION AS A **LAKE ERIE ENVIRONMENTAL MUSEUM**

Building Exteriors

- Replace exterior skin, doors, windows, wood canopies, downspouts and flashing to match original.
- Restore all roofs that have failed (except the garage) to match the existing.

Building Interiors

- Restore all interior walls, partitions, finishes, trim and cabinetry to reflect existing while meeting accessibility and program requirements.
- Reconfigure bathrooms and stairs to meet ADA requirements.

Structural

- Replace all wood joist and wood deck roof construction.

Accessibility

- Build a new handicap accessible ramp at the front entry of the Quarters Building.



Cleveland Heights School House – example of Historic Preservation of an Historic Landmark



SCHEME 1 HISTORICALLY PRESERVE THE COAST GUARD STATION AS A LAKE ERIE ENVIRONMENTAL MUSEUM

Program Elements

Quarters Building

- **Main Museum Exhibit Space** – 1st Floor (2,756 sf)
 - Exhibits displayed in various rooms portraying maritime history on the Great Lakes and Cuyahoga River, history of the Coast Guard Station site and architect, as well as boating and environmental education exhibits.
 - Various rooms to be restored to their original state and configuration.
- **Restaurant / Café Space** – 1st Floor (1,400 sf)
 - Large open dining area in the former Lounge and Mess Hall for café seating to serve museum visitors, park visitors and boaters.
 - Rebuild existing full-service kitchen to serve the dining area.
- **Observation Tower**
 - Opportunity for the public to experience exceptional panoramic views as a part of the museum tour.
- **Offices / Quarters Exhibit** – 2nd Floor (3,191 sf)
 - Support space for the museum and site.
 - Potential for quarters and dorm rooms to be restored to their original state and configuration and open to the public as a part of the museum tour.
- **Storage – Basement (1,216 sf)**
 - Storage space for the museum and site.
- **Main Entry / Lobby** – 1st Floor
 - Common entry providing access to the museum, restaurant and observation tower, containing ticket sales and public restrooms to serve the site.

SCHEME 1 HISTORICALLY PRESERVE THE COAST GUARD STATION AS A **LAKE ERIE ENVIRONMENTAL MUSEUM**

Boat House Building

- **Boat Room / Large Scale Exhibits** – (3,698 sf)
 - Large open room where historical boats and other large-scale displays can be exhibited as part of the museum tour.
 - Replace existing garage doors with glass overhead doors to allow open access, views and a seamless transition to outdoor exhibits.

Garage & Storage Building

- **Storage Space** – (967 sf)
 - Storage and support space for the site – not open to the public.



National Maritime Museum in San Francisco adjacent to Ghirardelli Square – example of a maritime museum as part of a larger public destination.

SCHEME 1 HISTORICALLY PRESERVE THE COAST GUARD STATION AS A **LAKE ERIE ENVIRONMENTAL MUSEUM**

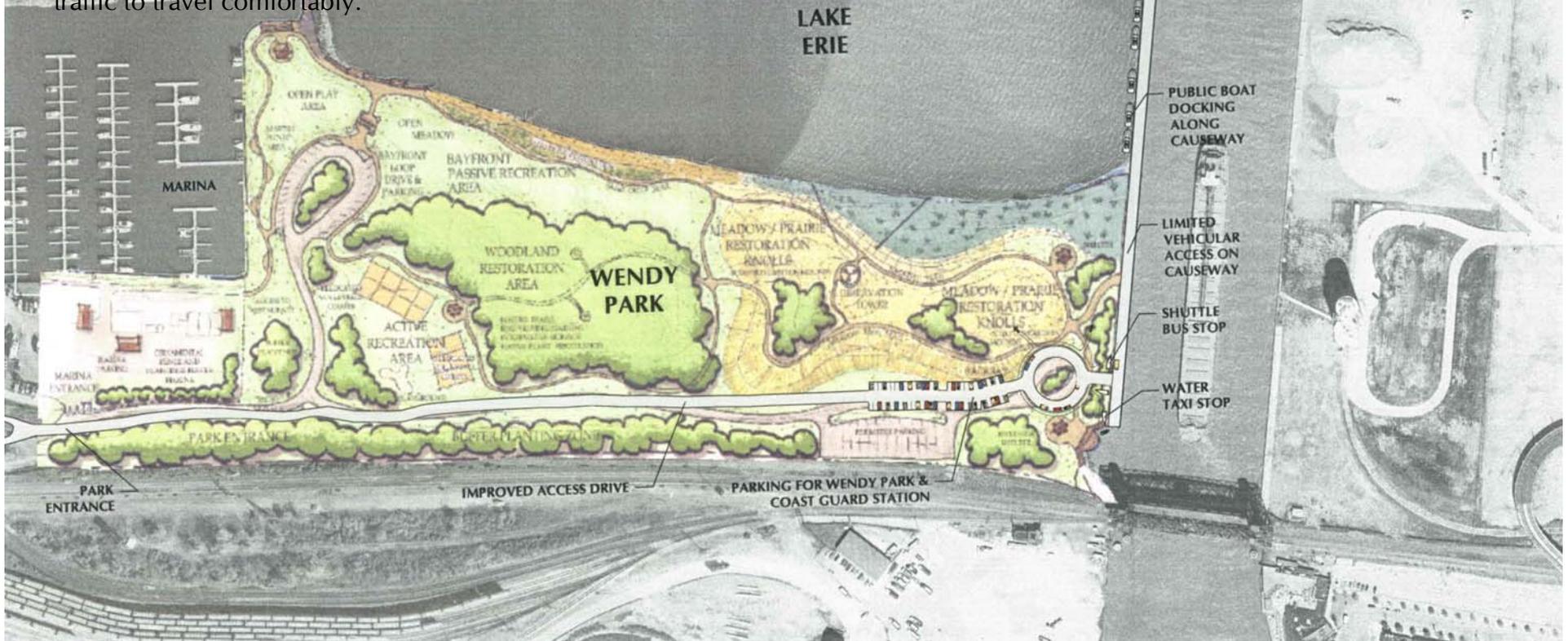
Site Components

- **Parking**
 - 13 total spaces on the Coast Guard Station site including 4 designated as handicap parking, 4 dedicated to service and delivery, and 5 reserved for employee parking.
 - Moderate parking required on Whiskey Island to serve the general public.
- **Shuttle Bus Drop-off**
 - Shuttle bus service from Whiskey Island parking to front entry of the museum.
- **Water Taxi Stop**
 - Water taxi system linked to other water taxi stops, including a new stop at the Whiskey Island parking lot.
- **Pedestrian Promenade**
 - 10'-0" walkway around perimeter of site for visitors to experience the site, get close to the water and enjoy panoramic views.
- **Park Space**
 - Open green space should be considered as park space for general public use and should tie back into Wendy Park. Includes thematic displays such as:
 - **Maritime Sculpture Garden** - Open green space with maritime sculpture pieces (anchors, small boats, etc.) for public viewing and interaction.
- **Historical Boat Marina**
 - Historical boats displayed in the existing boat docks south of the site as part of the historical maritime experience.
- **Public Boat Docking**
 - Allow public boat docking along the perimeter of the site and to the west of the causeway, creating a casual retreat and destination for boaters.

SCHEME 1 HISTORICALLY PRESERVE THE COAST GUARD STATION AS A **LAKE ERIE ENVIRONMENTAL MUSEUM**

Whiskey Island Improvements

Because the Coast Guard Station is so dependent and closely tied to Whiskey Island, a significant investment must occur at Whiskey Island in order to justify a large investment at the Coast Guard Station. A museum needs a vital community around it in order to sustain it and bring life to it. By implementing the proposed plan for Wendy Park and bringing organized passive and active recreation areas along with defined trails and restoration knolls, the museum and park can work together as a system, supporting and feeding off one another. Additional considerations should be given to providing moderate levels of parking to support the park and museum, water taxi and shuttle bus stops on the southeastern edge of Whiskey Island, and rebuilding and widening the causeway from 20'-0" wide to 25'-0" in order to allow limited vehicular and pedestrian traffic to travel comfortably.



SCHEME 1 HISTORICALLY PRESERVE THE COAST GUARD STATION AS A LAKE ERIE ENVIRONMENTAL MUSEUM

ADVANTAGES

1. **An Historic Landmark is saved**
2. **Provides the opportunity to use Historic Tax Credits to aid in the restoration**
3. **Supports the Waterfront District Plan, Ohio & Erie Canalway and Whiskey Island Park Plan**
4. **Creates a unique destination for boaters**
5. **Tells the history of the Station and Cleveland's Lake and River**
6. **Accessible through interesting and exciting means**

& DISADVANTAGES

1. **Significant restoration cost**
2. **Requires a Non-Profit Partner to run Museum and fundraise to sustain it**
3. **Remote location**
4. **Strict Preservation Guidelines must be followed**
5. **Limited Access with no direct parking**
6. **A Museum on its own may not be enough to spur substantial activity**

SCHEME 1

HISTORICALLY PRESERVE THE COAST GUARD STATION AS A **LAKE ERIE ENVIRONMENTAL MUSEUM**

PROGRAM COST ESTIMATE

TOTAL ESTIMATED PROJECT DEVELOPMENT COST =

\$8,036,000

Number	Item	Cost / SF	Cost / LF	Cost / Item	Units	Sub Item Total	Item Total
1 Site Components							\$ 3,455,610
1.0	Demolition & Removal	\$0	\$0	\$200,000	1 LS	\$200,000	
1.1	Causeway @ 25'	\$0	\$850	\$0	1435 LF	\$1,219,750	
1.2	Site Utilities					\$625,000	
1.2.1	Conventional Utility Connections	\$0	\$250	\$0	1500 LF	\$375,000	
1.2.2	Green Self-sufficient Equipment	\$0	\$0	\$250,000	1 LS	\$250,000	
1.3	Vehicle Pavements	\$3.25	\$0	\$0	11000 SF	\$35,750	
1.4	Pedestrian Pavements					\$84,600	
1.4.1	Concrete	\$6	\$0	\$0	6300 SF	\$37,800	
1.4.2	Special Paving	\$9	\$0	\$0	5200 SF	\$46,800	
1.5	Landscape					\$38,310	
1.5.1	Topsoil / Seeding	\$0.87	\$0	\$0	13000 SF	\$11,310	
1.5.2	Trees	\$0	\$0	\$600	31 EA	\$18,600	
1.5.3	Planting Beds / Shrubs	\$3	\$0	\$0	2800 SF	\$8,400	
1.6	Historic Boat Marina	\$0	\$0	\$5,000	6 Slip	\$30,000	
1.7	Public Boat Docking	\$0	\$300	\$0	324 LF	\$97,200	
1.8	Retaining Wall / Sheet Piling Replacement	\$75	\$0	\$0	15000 SF	\$1,125,000	
2 Quarters Building							\$ 1,344,135
2.1	Exterior Restoration / Reconstruction					\$422,500	
2.1.1	Walls	\$25	\$0	\$0	9700 SF	\$242,500	
2.1.2	Roof	\$25	\$0	\$0	4200 SF	\$105,000	
2.1.3	Openings	\$0	\$0	\$75,000	1 LS	\$75,000	
2.2	Main Entry / Lobby	\$85	\$0	\$0	1500 SF	\$127,500	
2.3	Museum Exhibit Space	\$85	\$0	\$0	2756 SF	\$234,260	
2.4	Restaurant / Café Space	\$125	\$0	\$0	1400 SF	\$175,000	
2.5	Observation Tower	\$65	\$0	\$0	1000 SF	\$65,000	
2.6	Offices / Quarters Exhibit (2nd Flr)	\$85	\$0	\$0	3191 SF	\$271,235	
2.7	Storage (Basement)	\$40	\$0	\$0	1216 SF	\$48,640	
3 Boat House Building							\$ 469,880
3.1	Exterior Restoration / Reconstruction					\$248,000	
3.1.1	Walls	\$25	\$0	\$0	4700 SF	\$117,500	
3.1.2	Roof	\$25	\$0	\$0	3700 SF	\$92,500	
3.1.3	Openings	\$0	\$0	\$38,000	1 LS	\$38,000	
3.2	Boat Room / Large Exhibit	\$60	\$0	\$0	3698 SF	\$221,880	
4 Storage Building							\$ 131,680
4.1	Exterior Restoration / Reconstruction					\$93,000	
4.1.1	Walls	\$25	\$0	\$0	2000 SF	\$50,000	
4.1.2	Roof	\$25	\$0	\$0	1000 SF	\$25,000	
4.1.3	Openings	\$0	\$0	\$18,000	1 LS	\$18,000	
4.2	Interior Storage	\$40	\$0	\$0	967 SF	\$38,680	
SUB-TOTAL - Construction Labor and Materials							\$ 5,401,305
Contractor's Overhead & Profit							15.0%
							\$ 810,196
TOTAL - Construction Cost							\$ 6,211,501
Contingency							15.0%
							\$ 931,725
Fees & Administrative Expenses							12.5%
							\$ 892,903
TOTAL - Estimated Project Development Cost							\$ 8,036,129

*Exhibit Space costs are not included in the above cost estimate.

SCHEME 1

HISTORICALLY PRESERVE THE COAST GUARD STATION AS A LAKE ERIE ENVIRONMENTAL MUSEUM

The Development Plan

The second option combines public, educational, commercial and recreational uses in a self-sustaining green place. The main intention of this scheme is to develop the site of the Old Coast Guard Station to its fullest potential, creating an active and diverse destination for people of all ages throughout the entire region. Building on the first option where the site was restored to its original state and served as a Lake Erie Environmental Museum, this alternative complements and enhances this idea of utilizing the Coast Guard Station as a teachable monument with an Environmental Educational Center and “Floating Schoolhouse,” a full-service restaurant, banquet space, water activity center and an interactive park space. This dynamic mixture of uses is meant to generate a synergy that provides public access to the waterfront in a truly unique way.

Similarly to the first scheme, a Lake Erie Environmental Museum would occupy the Quarters Building, preserving the original state of the building as much as possible so that visitors can learn through both exhibits and experiencing this historical landmark. In addition, the site itself would serve as a sustainable showcase, giving visitors the chance to observe examples of green technology and how they work to create sustainable systems. The Coast Guard Station as a teachable monument would be further enhanced through an Environmental Educational Center located in the Garage and Storage Building and a “Floating Schoolhouse” docked along the site. The concept for this educational boat is to provide an interesting and hands-on way to teach children, families and boaters about the history of Lake Erie and the Cuyahoga River as well as environmental issues that affect our water and city. The “Floating Schoolhouse” not only provides another facet of learning to the site, but also provides the opportunity to connect the Coast Guard Station to the North Coast Harbor and related facilities like the Great Lakes Science Center by potentially serving as a ferryboat between the two sites, enhancing and expanding the educational experience.

An important aspect of this site is its ability to serve as a destination, retreat and casual encounter for boaters. By building boardwalks and boat docks along the entire site and causeway, a public haven for boaters is created that does not currently exist in Cleveland, with opportunities for service, entertainment and recreation. The restaurant, located in the old Boat House building would serve as a major draw and source of activity, catering to boaters who are spending time at the site or stopping by for quick take-out service. In addition, the activity center would house a shop for boaters, allowing them to dock and pick up necessities like food and supplies.

SCHEME 2 DEVELOP THE COAST GUARD STATION SITE TO ITS FULLEST POTENTIAL WITH **COMMERCIAL & PUBLIC USES**

Another component of the site which does not currently exist in Cleveland is the Waterfront Activity Center. Taking advantage of the protected harbor, a unique opportunity exists to provide boating opportunities and water activities to people that do not own boats or have private access to the water, including bike, paddle boat and sailboat rental to name a few. This use helps contribute to the ability of the Old Coast Guard Station site in becoming a recreational hub and source for people to connect with the waterfront.

The open park space that is created at this site is another layer that adds to the site's ability to educate and entertain. The exterior environment is an extension of all the programs that make up the site, from the dining patios that spill out from the restaurant and banquet space to the amphitheatre that can be used as an outdoor classroom for the Environmental Educational Center, to the outdoor exhibits of maritime artifacts and historical boats. All of the activities of the Coast Guard Station can take advantage of their unique location and ability to experience the waterfront.

As in the first option, the site is accessed by boat, water taxi or shuttle bus. The pedestrian and bike experience is further enhanced in this scheme with a boardwalk constructed along the length of the original causeway, strengthening the Coast Guard Station's connection to Whiskey Island. This scheme, more than any other, depends on this association and the ability to work together to create an exciting and one of a kind regional draw.

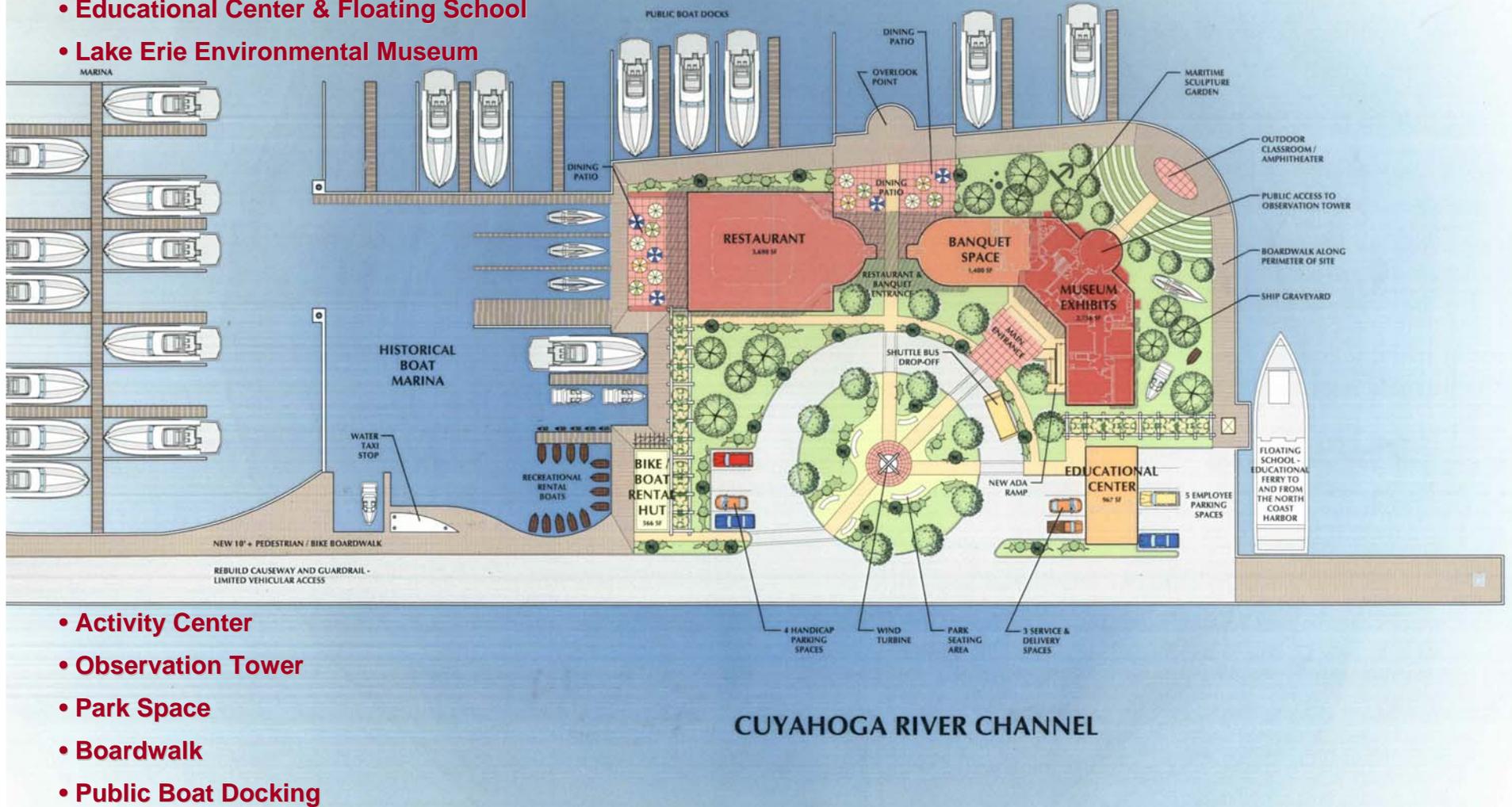
Because of the mixed-use environment that this alternative presents, the Coast Guard Station has the ability to remain an active destination throughout the day, night and year. However, since much of the activities are seasonal in nature and most access to the site is by boat, the winter months would be far less active, with limited hours for the restaurant and museum.

This option more than any other supports the Lakefront Plan, Whiskey Island Park and Ohio & Erie Canal Corridor, as well as creating a vibrant and dynamic mixed-use destination that develops a unique and lively synergy between history, sustainability, education, recreation, and entertainment.

SCHEME 2 DEVELOP THE COAST GUARD STATION SITE TO ITS FULLEST POTENTIAL WITH **COMMERCIAL & PUBLIC USES**

Key Program Elements

- Renovation and Adaptive Re-Use of Existing Building & Site
- New Restaurant & Banquet Space
- Educational Center & Floating School
- Lake Erie Environmental Museum



- Activity Center
- Observation Tower
- Park Space
- Boardwalk
- Public Boat Docking

SCHEME 2 DEVELOP THE COAST GUARD STATION SITE TO ITS FULLEST POTENTIAL WITH **COMMERCIAL & PUBLIC USES**

What would this entail?

Causeway / Roadway

- Rebuild cartway to its existing width of 20'-0", replacing curb and guardrail to allow for 2-way limited vehicular access.
- Create a 10'-0" pedestrian promenade / bike path on the west side of the causeway, establishing a pleasant and significant pedestrian connection to the Coast Guard Station.
- Vehicular traffic on the causeway would be limited to shuttle buses, ADA accessibility, limited employee traffic, service and deliveries, and emergency vehicles.
- Build a small transient marina (approximately 36 slips) off of the pedestrian promenade to the west of the causeway for the public to serve as a boat "parking lot" for the Coast Guard Station and Whiskey Island.

Site

- Clear overgrown brush and deteriorated pavement.
- Rebuild circular drive with additional parking areas.
- Create a 10'-0" boardwalk along the entire perimeter of the site, connecting back into the pedestrian promenade along the causeway, as well as additional connective sidewalks and crosswalks with loggias along strategic paths.
- Establish main entries and dining patios with special paving.
- Enhance the site with landscaping in all open areas to create a park-like atmosphere that ties into the plan developed for Wendy Park. Landscaping should reflect the maritime, educational and environmental conceptual theme for the site (i.e.: maritime sculpture garden, ship graveyard, etc.)

Site Utilities

- Rebuild all utilities, taking the site "off the grid" and retrofitting green technology such as geothermal, solar power, wind power, satellite generator program by Cleveland Public Power, etc. so that the buildings and systems can be 100% sustainable, serving as a sustainable showcase to educate visitors.

SCHEME 2 DEVELOP THE COAST GUARD STATION SITE TO ITS FULLEST POTENTIAL WITH **COMMERCIAL & PUBLIC USES**

Building Exteriors

- Replace exterior skin, doors, windows, wood canopies, downspouts and flashing to accommodate new programs and match original where feasible.
- Restore all roofs that have failed (except the garage) to accommodate new programs and match existing where feasible.

Building Interiors

- Design new interiors to accommodate the new programs, maintaining historical elements and character where possible, especially in the museum section of the Quarters Building.

Structural

- Replace all wood joist and wood deck roof construction.

Accessibility

- Build a new handicap accessible ramp at the front entry of the Quarters Building, and meet ADA requirements throughout the entire site and buildings.

Examples of festive waterfront destinations that revolve around a Historic Landmark



Ghirardelli Square – San Francisco, CA



Faneuil Hall – Boston, MA



SCHEME 2 DEVELOP THE COAST GUARD STATION SITE TO ITS FULLEST POTENTIAL WITH **COMMERCIAL & PUBLIC USES**

Program Elements

Quarters Building

- **Main Museum Exhibit Space** – 1st Floor (2,756 sf)
 - Exhibits portraying maritime history of the Great Lakes and the Cuyahoga River, history of the Coast Guard, building, site and architect would be setup in various rooms that are reflective of their original state while combining rooms from the original layout to accommodate larger exhibits.
 - Setup boater and environmental educational exhibits, connecting the Coast Guard Station to the environmental concept for Whiskey Island and creating a 21st century educational place for kids. Exhibits could include water-monitoring, green technology showcase, seminars for boaters, etc.
- **Banquet Space** – 1st Floor (1,400 sf)
 - Large open room to accommodate special events as well as large museum exhibits.
 - Serviced by the restaurant kitchen in the adjacent Boat House Building.
 - Accessible from both main entry to the museum and the west entry shared with the restaurant.
 - Ability for banquet space to spill-out into outdoor patio space.
- **Observation Tower**
 - Opportunity for the public to experience exceptional panoramic views as a part of the museum tour.
 - Possibility to light up the tower like a lighthouse.
- **Offices / Quarters Exhibit** – 2nd Floor (3,191 sf)
 - Support space for the museum, restaurant, educational center and site.
 - Quarters and dorm rooms could be restored to their original state and configuration and open to the public as a part of the museum tour.
- **Storage** – Basement (1,216 sf)
 - Storage space for the museum and site.

SCHEME 2 DEVELOP THE COAST GUARD STATION SITE TO ITS FULLEST POTENTIAL WITH **COMMERCIAL & PUBLIC USES**

- **Main Entry / Lobby** – 1st Floor
 - Common entry that provides access to the museum, banquet space, observation tower, offices and rear open classroom / amphitheater space, containing ticket sales and public restrooms to serve the site.

Boat House Building

- **Restaurant** – (3,698 sf)
 - Full-service restaurant with a kitchen, large bar and outdoor seating.
 - Ability for a mezzanine level to provide additional seating.
 - Caters to boaters, both dine-in and take-out – ability to provide “drive-through” take-out service to boaters at the adjacent docks.
 - 2 large outdoor dining patios surround the restaurant.
 - Replace existing garage doors with glass overhead doors that can be kept open during nice weather to allow for a seamless transition to outdoor dining, and provide views when closed.

Garage & Storage Building

- **Educational Center** – (967 sf)
 - Offices, ticket sales and support space for the “Floating School.”
 - 1 garage bay would serve as the open ticket counter, which visitors would have to pass before walking through the loggia to get to the “Floating School.”

New Site Program Elements

- **Floating School** – (100’ – 150’ in length)
 - Educational boat / ferry docked behind the Garage & Storage Building on the north side of the site.
 - Educates school children, families and boaters on the water quality, environmentalism and history of the Great Lakes and Cuyahoga River.

SCHEME 2 DEVELOP THE COAST GUARD STATION SITE TO ITS FULLEST POTENTIAL WITH **COMMERCIAL & PUBLIC USES**

- Possibility to commute to and from the North Coast Harbor, integrating the Coast Guard Station and its connection to history and education to the experience of the Great Lakes Science Center.
- **Boat / Bike Rental Hut** (566 sf)
 - Activity / recreational center at the end of the causeway and open to the public for bike, paddle boat, jet ski, kayak, roller blade, sailboat, small boat, etc. rental.
 - Provides instructional access and training for boating and water activities for people who could not otherwise afford access to or have the opportunity to experience boating.
 - Serves as a boater concession stand, allowing boaters to temporarily dock at the site and pick up food, supplies, etc.
 - Possibility for community services for kids.

The New Hudson River School – Example of a ‘Floating Schoolhouse’ in New York

HUDSON RIVER SLOOP CLEARWATER

ENVIRONMENTAL EDUCATION PROGRAMS

THE NEW HUDSON RIVER SCHOOL

A DAY ON THE CLEARWATER ... SKILLS FOR A LIFETIME

You probably started your voyage on *Clearwater* from a pier or dock along the river's edge. The *Clearwater* sails from communities as far north as Albany and as far south as New York Harbor and Long Island Sound.

Not all communities have docks that *Clearwater* can use. The water might be too shallow, there might be a bridge that is too low for *Clearwater's* mast — or the community might not have facilities for the public to enjoy its legal right of access to its river. *Do you think it is important for the public to have access to the river?*

CHECK OFF THE ACTIVITIES YOU PERFORMED AND THE SKILLS YOU LEARNED:

- Helped raise the 3,000-pound mainsail.
- Identified the physical dimensions of the sloop *Clearwater*.
- Learned about the importance of having free public access to our river.
- Helped set, tow, and retrieve the trawl net.
- Defined ESTUARY, SLOOP and WATERSHED.
- Learned how fish breathe and sense things under water.
- Examined fish and other marine life in an aquarium.
- Examined plankton with magnifiers.
- Went below deck into the main cabin.
- Learned how the crew lives in comfortable simplicity.
- Located the sloop's position on a nautical chart.
- Determined direction with a compass.
- Identified marks on a chart and saw the real features they represent.
- Steered the sloop!
- Spent a few moments in quiet, just looking and listening.
- Sang a sea chanty or song about the river and nature.
- Learned about pollution — and solutions — in the river.
- Talked about what people can do to make the river cleaner.

PERFORMED ONE OR MORE OF THESE WATER OBSERVATIONS:

- Dissolved Oxygen
- Salinity
- pH
- Temperature
- Turbidity
- Depth

This young sturgeon is alive and well, and will shortly be returned to the river.

R D F I E L D T R I P S

2. Sail Raising. Nothing can compare with the thrill of all students working as a team to raise a 3,000-pound mainsail—no watches, no rigging, — just the power of cooperation. Then the sloop is turned off and the boat stowed away, harnessing the power of the wind!

3. Water Monitoring and Investigative Studies. The water's food web is enriched by a rich store of small organisms, which need clean water to grow. Students use water monitoring equipment to determine the chemical and physical health of the river. They collect plankton nets and analyze them in a laboratory to see the Hudson's smaller inhabitants.

4. History and Music. Through music, art, and education *Clearwater* educators help students gain a sense of the people and culture that have thrived on the banks of the Hudson. It is necessary to understand what's happened to the river in the past in order to chart a course for the future.

5. Navigation and Steering. Once the hauling is over, the school has just begun. Students

6. Continued Learning Stations. Counselor provides a number of related inquiry activities to show the school has just begun. Students

SCHEME 2 DEVELOP THE COAST GUARD STATION SITE TO ITS FULLEST POTENTIAL WITH COMMERCIAL & PUBLIC USES

Site Components

- **Parking**
 - 12 total spaces on the Coast Guard Station site including 4 designated as handicap parking, 3 dedicated to service and delivery, and 5 reserved for employee parking.
 - Moderate parking required on Whiskey Island to serve the general public.
- **Shuttle Bus Drop-off**
 - Shuttle bus service from Whiskey Island parking to front entry of the museum.
- **Water Taxi Stop**
 - Water taxi system linked to other water taxi stops, including a new stop at the Whiskey Island parking lot. Includes an open-air structure for people to sit and wait.
- **Boardwalk**
 - 10'-0" boardwalk to be built around the entire perimeter of the site and causeway, providing visitors with the ability to get close to and experience the water and boating activity.
- **Loggias**
 - Connective elements in the style of the Coast Guard Building.
 - One connects entry to site and bike/boat rental to the restaurant and outdoor patio space while the other connects the ticket window from the Educational Center to the Floating School.
- **Outdoor Classroom**
 - Amphitheater that cascades down from the rear entry of the Quarters Building onto a stage.
 - Used for educational lectures for the museum and educational center.

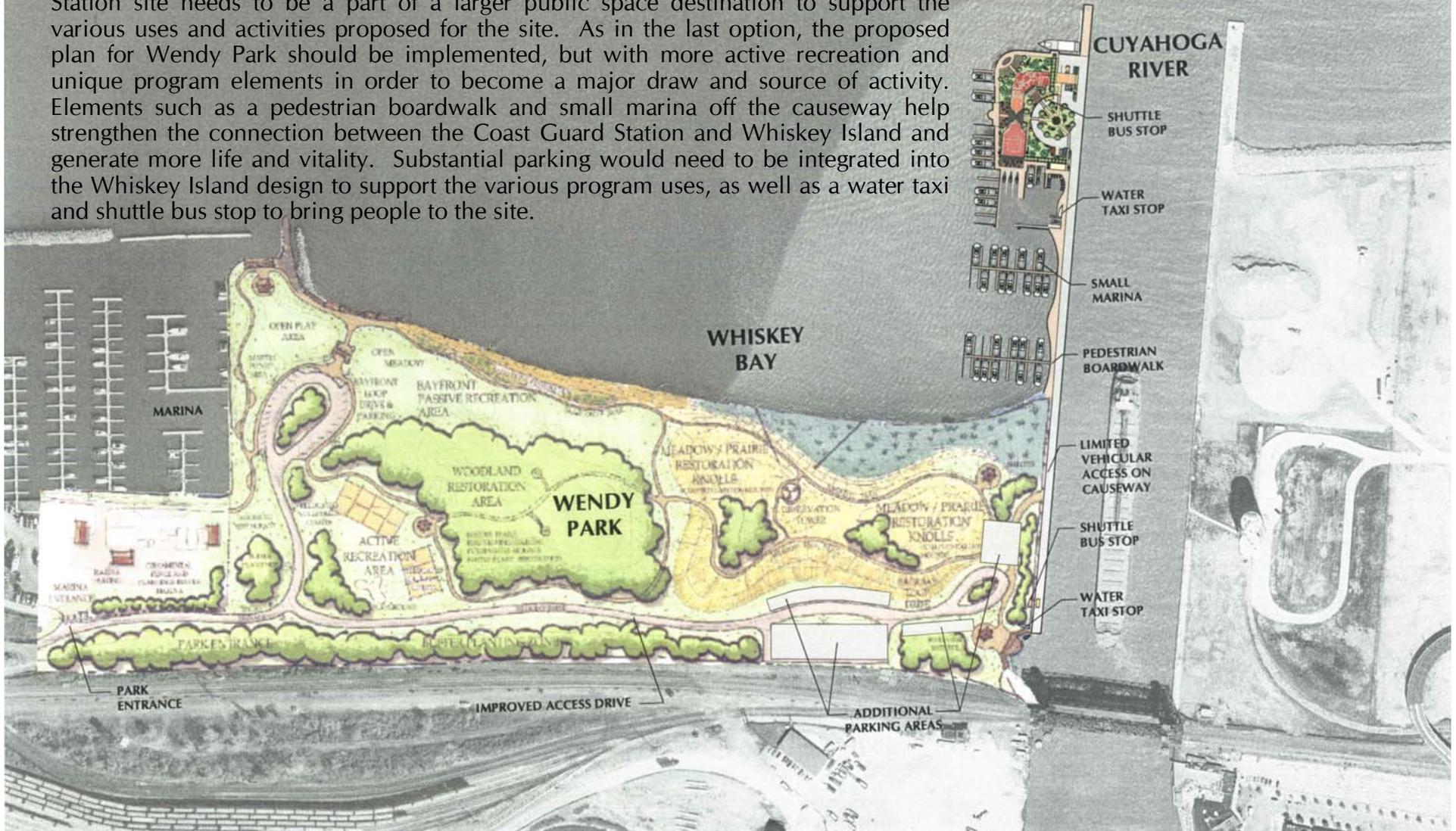
SCHEME 2 DEVELOP THE COAST GUARD STATION SITE TO ITS FULLEST POTENTIAL WITH **COMMERCIAL & PUBLIC USES**

- **Park Space**
 - Open green space should be considered as park space for general public use and should tie back into Wendy Park. Includes thematic displays such as:
 - **Maritime Sculpture Garden** - Open green space with maritime sculpture pieces (anchors, small boats, etc.) for public viewing and interaction.
 - **Ship Grave Yard** - Open green space with historical boats scattered about for public viewing and interaction.
- **Wind Turbine**
 - Located at the center of the circle and provides a power source for the site, tying into the environmental theme of Whiskey Island.
- **Historical Boat Marina**
 - Historical boats displayed in the existing boat docks south of the site as part of the museum experience.
- **Public Boat Docking**
 - New boat docks to be built along the west side of the site and causeway for public docking (49 potential spaces).

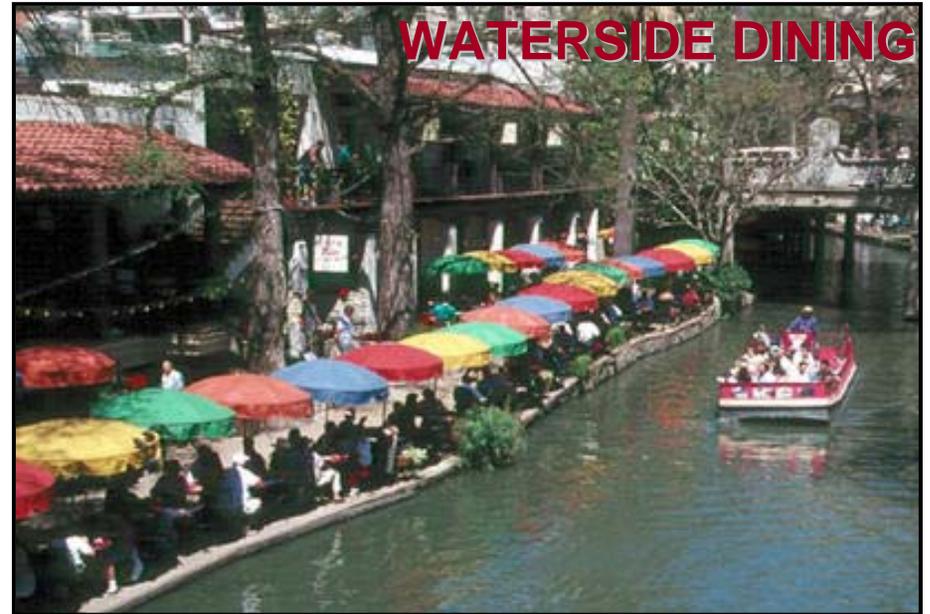
SCHEME 2 DEVELOP THE COAST GUARD STATION SITE TO ITS FULLEST POTENTIAL WITH **COMMERCIAL & PUBLIC USES**

Whiskey Island Improvements

In order for this scheme to become a festive waterfront destination, the Coast Guard Station site needs to be a part of a larger public space destination to support the various uses and activities proposed for the site. As in the last option, the proposed plan for Wendy Park should be implemented, but with more active recreation and unique program elements in order to become a major draw and source of activity. Elements such as a pedestrian boardwalk and small marina off the causeway help strengthen the connection between the Coast Guard Station and Whiskey Island and generate more life and vitality. Substantial parking would need to be integrated into the Whiskey Island design to support the various program uses, as well as a water taxi and shuttle bus stop to bring people to the site.



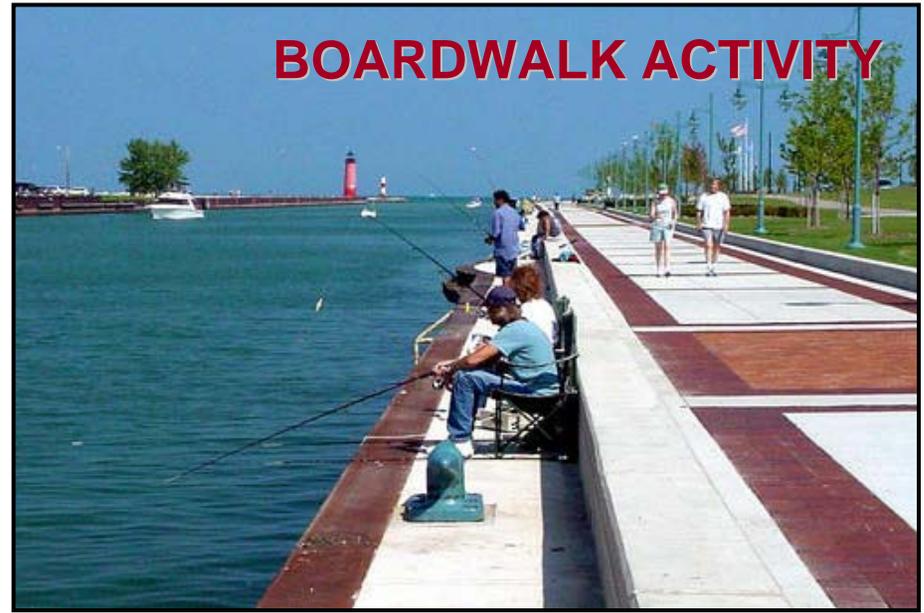
SCHEME 2 DEVELOP THE COAST GUARD STATION SITE TO ITS FULLEST POTENTIAL WITH **COMMERCIAL & PUBLIC USES**



SCHEME 2 DEVELOP THE COAST GUARD STATION SITE TO ITS FULLEST POTENTIAL WITH **COMMERCIAL & PUBLIC USES**



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BOATING RETREAT



SCHEME 2 DEVELOP THE COAST GUARD STATION SITE TO ITS FULLEST POTENTIAL WITH **COMMERCIAL & PUBLIC USES**

ADVANTAGES

1. **Mixed-Uses create a unique & active destination**
2. **Brings people to the water in various ways**
3. **Strongly supports the Waterfront District Plan, Ohio & Erie Canalway and Whiskey Island Park Plan**
4. **Creates a unique destination / refuge for boaters**
5. **Links the site to the North Coast Harbor**
6. **Mixture of uses allows for year-round usage of site**
7. **Ties education, sustainability, commercial & recreational uses together**

& DISADVANTAGES

1. **Major investment**
2. **Requires Commercial and/or Non-Profit partners to operate Restaurant and Museum**
3. **Remote location denies program elements from feeding off of other destinations**
4. **Historic buildings will be altered from their historic state**
5. **Limited access and no direct parking**
6. **Minimal activity in off-season**
7. **Whiskey Island must become an active destination and regional draw**

SCHEME 2 DEVELOP THE COAST GUARD STATION SITE TO ITS FULLEST POTENTIAL WITH **COMMERCIAL & PUBLIC USES**

PROGRAM COST ESTIMATE

TOTAL ESTIMATED PROJECT DEVELOPMENT COST =

\$9,239,000

Number	Item	Cost / SF	Cost / LF	Cost / Item	Units	Sub Item Total	Item Total
1 Site Components							\$ 3,989,270
1.0	Demolition & Removal	\$0	\$0	\$200,000	1 LS	\$200,000	
1.1	Causeway @ 25'	\$0	\$850	\$0	1435 LF	\$1,219,750	
1.2	Site Utilities					\$625,000	
1.2.1	Conventional Utility Connections	\$0	\$250	\$0	1500 LF	\$375,000	
1.2.2	Green Self-sufficient Equipment	\$0	\$0	\$250,000	1 LS	\$250,000	
1.3	Retaining Wall / Sheet Piling Replacement	\$75	\$0	\$0	15000 SF	\$1,125,000	
1.4	Vehicle Pavements	\$3.25	\$0	\$0	11000 SF	\$35,750	
1.5	Pedestrian Pavements					\$65,400	
1.5.1	Concrete	\$6	\$0	\$0	5200 SF	\$31,200	
1.5.2	Special Paving	\$9	\$0	\$0	3800 SF	\$34,200	
1.6	Landscape					\$47,370	
1.6.1	Topsoil / Seeding	\$0.87	\$0	\$0	11000 SF	\$9,570	
1.6.2	Trees	\$0	\$0	\$600	50 EA	\$30,000	
1.6.3	Planting Beds / Shrubs	\$3	\$0	\$0	2600 SF	\$7,800	
1.7	Historic Boat Marina	\$0	\$0	\$5,000	7 Slip	\$35,000	
1.8	Public Boat Docking					\$245,000	
1.8.1	Slips	\$0	\$0	\$5,000	49 Slip	\$245,000	
1.9	Water Taxi Stop Pavilion	\$0	\$0	\$2,500	1 LS	\$2,500	
1.10	Seating / Benches	\$0	\$0	\$500	5 EA	\$2,500	
1.11	Loggias	\$0	\$100	\$0	168 LF	\$16,800	
1.12	Outdoor Classroom / Amphitheater	\$8	\$0	\$0	2000 SF	\$16,000	
1.13	Boardwalk					\$353,200	
1.13.1	Overbuild (Soil / Causeway beneath)	\$12	\$0	\$0	5600 SF	\$67,200	
1.13.2	Extended (Water beneath)	\$55	\$0	\$0	5200 SF	\$286,000	
2 Quarters Building							\$ 1,288,135
2.1	Exterior Restoration / Reconstruction					\$422,500	
2.1.1	Walls	\$25	\$0	\$0	9700 SF	\$242,500	
2.1.2	Roof	\$25	\$0	\$0	4200 SF	\$105,000	
2.1.3	Openings	\$0	\$0	\$75,000	1 LS	\$75,000	
2.2	Main Entry / Lobby	\$85	\$0	\$0	1500 SF	\$127,500	
2.3	Museum Exhibit Space	\$85	\$0	\$0	2756 SF	\$234,260	
2.4	Banquet Space	\$85	\$0	\$0	1400 SF	\$119,000	
2.5	Observation Tower	\$65	\$0	\$0	1000 SF	\$65,000	
2.6	Offices / Quarters Exhibit (2nd Flr)	\$85	\$0	\$0	3191 SF	\$271,235	
2.7	Storage (Basement)	\$40	\$0	\$0	1216 SF	\$48,640	
3 Boat House Building							\$ 710,250
3.1	Exterior Restoration / Reconstruction					\$248,000	
3.1.1	Walls	\$25	\$0	\$0	4700 SF	\$117,500	
3.1.2	Roof	\$25	\$0	\$0	3700 SF	\$92,500	
3.1.3	Openings	\$0	\$0	\$38,000	1 LS	\$38,000	
3.2	Restaurant	\$125	\$0	\$0	3698 SF	\$462,250	
4 Storage Building							\$ 165,525
4.1	Exterior Restoration / Reconstruction					\$93,000	
4.1.1	Walls	\$25	\$0	\$0	2000 SF	\$50,000	
4.1.2	Roof	\$25	\$0	\$0	1000 SF	\$25,000	
4.1.3	Openings	\$0	\$0	\$18,000	1 LS	\$18,000	
4.2	Educational Center	\$75	\$0	\$0	967 SF	\$72,525	
5 Bike / Boat Rental							\$ 56,600
5.1	New Construction	\$100	\$0	\$0	566 SF	\$56,600	
SUB-TOTAL - Construction Labor and Materials							\$ 6,209,780
Contractor's Overhead & Profit							15.0%
							\$ 931,467
TOTAL - Construction Cost							\$ 7,141,247
Contingency							15.0%
							\$ 1,071,187
Fees & Administrative Expenses							12.5%
							\$ 1,026,554
TOTAL - Estimated Project Development Cost							\$ 9,238,988

*Exhibit Space costs are not included in the above cost estimate.

*Floating School costs are not included in the above cost estimate.

SCHEME 2 DEVELOP THE COAST GUARD STATION SITE TO ITS FULLEST POTENTIAL WITH **COMMERCIAL & PUBLIC USES**

The Development Plan

The third option symbolically preserves the Old Coast Guard Station in a unique and natural haven that serves as a reminder of the site's history. The essence of the Coast Guard Station building serves as a backdrop for integrating history, sustainability, nature and recreation in a park setting, creating an experience rich in memories, awareness and associations.

All that remains of the Old Coast Guard Station is the integral structure and essence of the Quarters Building, the main visual and programmatic element of the site. All organic and extraneous parts of the Coast Guard Station, including the Boat House, Garage, roofs, windows and doors would be cleared from the site so that the primary shell is all that is left. The space is transformed into open-air gardens in a ruin-like setting that tells the story of the Coast Guard Station and maritime history throughout the various spaces that remain. A system of paths meander through the site with "story-tellers" setup along the way that describe the various habitats and exhibits, allowing visitors to experience the site both casually and formally. The park itself serves as an instrument to educate visitors on maritime history and sustainability, including installations of green technology, maritime artifacts, historical boats and natural habitats.

The Water Activity Center would be a positive enhancement to this scheme, serving as a source of activity to the site. As in the second option, the recreation center would allow visitors to rent bikes, boats and other activities that bring life to the waterfront.

Unlike the other alternatives proposed for the Coast Guard Station, this option does not incorporate vehicular access. One must travel by boat, water taxi, bike or walk to the site along the causeway, supporting the natural and island-like feel of the site. Boats are able to dock along the site and causeway, creating an easy and open refuge for boaters. Access to the site would be limited from spring to fall during the day in order to assure security and safety.

The goal of this scheme was based on opening the site to the public as much as possible while minimizing restoration costs. By maintaining only the walls of the Quarters Building, the initial and ongoing investment is less substantial than the other options while saving the essence of the Coast Guard Station and creating a unique destination.

This option can be viewed as an extension of Whiskey Island where a natural retreat is created around a beautiful historic landmark. Through this transformation, the Old Coast Guard Station becomes a place to remember the past and learn about environmental issues that affect the future of our water and city.

SCHEME 3 CREATE AN EDUCATIONAL PASSIVE PARK WITH MONUMENTAL RUINS OF THE COAST GUARD STATION

Key Program Elements

- Preservation of Main Coast Guard Station Structure
- Removal of Adjacent Buildings
- Maritime History / Environmental Learning Trail
- Park Space
- Observation Tower
- Activity Center
- Public Boat Docking



SCHEME 3 CREATE AN **EDUCATIONAL PASSIVE PARK** WITH **MONUMENTAL RUINS** OF THE COAST GUARD STATION

What would this entail?

Causeway / Roadway

- Rebuild cartway to its existing width of 20'-0", replacing the curb and guardrail to sustain pedestrian and bike traffic only (no vehicular traffic except emergency vehicles).
- Install movable bollards at the beginning of the causeway to allow emergency vehicles to access the site.
- Causeway should be covered with a natural material, such as gravel or crushed stone.
- Install bollards along the west face of the causeway to allow public boat docking along the entire length.

Site

- Clear site of all unnatural elements to prepare for a natural park landscape.
- Create meandering paths made out of gravel / crushed stone that branch off of the causeway and guide visitors around the site.
- Establish different habitats and landscapes throughout the site.

Site Utilities

- Minimal utilities needed, such as lighting which would be provided by the wind turbine or solar power generators.

Building Exteriors

- Demolish the Boat House and Garage so just the Quarters building, the main component of the Coast Guard Station remains.
- Remove all extraneous and "perishable" parts of the building that have not lasted or are not permanent such as wood components, windows, doors, roofs, etc. so that all that is left is the integral shell and essence of the building.
- Patch all concrete and treat all surfaces to sustain the exterior environment.
- Allow greenery and natural landscaping to grow inside and on the building walls, creating "overgrown ruins."

SCHEME 3 CREATE AN **EDUCATIONAL PASSIVE PARK** WITH **MONUMENTAL RUINS** OF THE COAST GUARD STATION

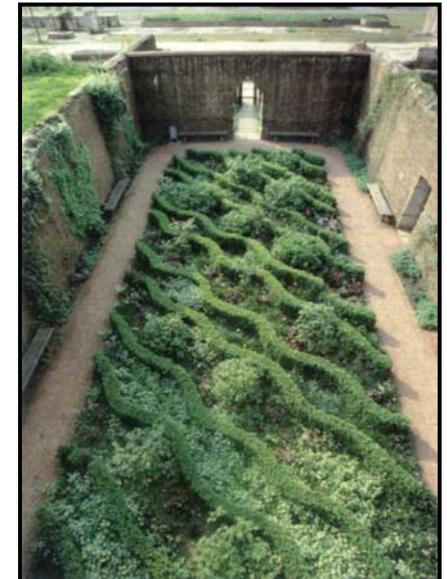
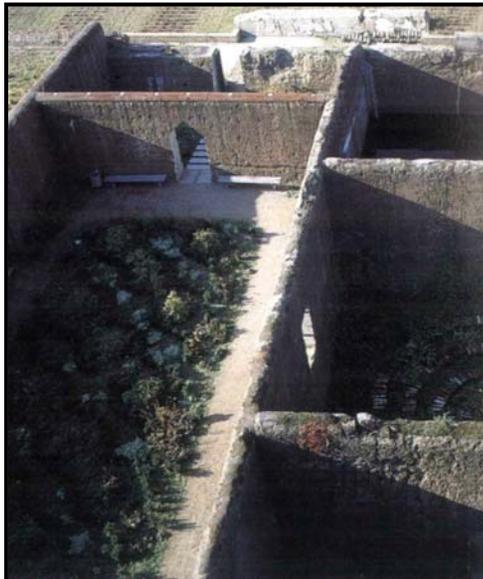
Building Interiors

- Remove all extraneous and “perishable” components.
- Treat all remaining surfaces to withstand the external environment.
- Allow landscaping and greenery to infiltrate the spaces.
- Space is defined by walls alone and is open to the sky, except for the observation tower, which will remain in tact and accessible to provide views from the top of the tower if that access can be provided in a safe manner.

Accessibility

- Build trails to accommodate accessibility.

Landschaftspark – Duisburg-Nord - Former steelworks transformed into a public park that combines industrial heritage and nature



SCHEME 3 CREATE AN **EDUCATIONAL PASSIVE PARK** WITH **MONUMENTAL RUINS** OF THE COAST GUARD STATION

Program Elements

Maritime / Environmental History / Learning Trail

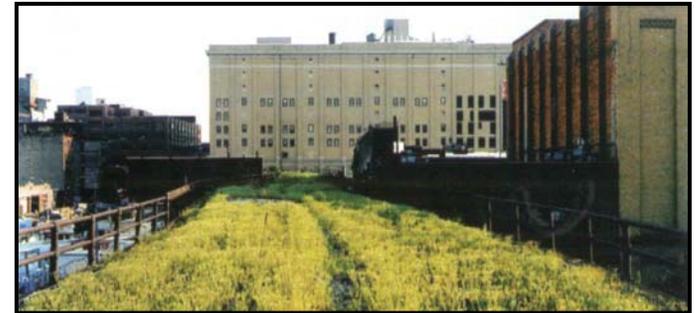
- A meandering trail with “story-tellers” setup along the path system that educates visitors and describes the various components of the site, with the ability for organized nature / eco-tours.
- Various habitats and exhibits are created with distinct characters and landscapes in various “zones” throughout the site.
- “Story-tellers” would be clearly defined areas or extensions of the trail with benches, viewports and plaques that describe and educate visitors on the components of the various zones.
- Trails would be made out of gravel / crushed stone.
- Possible “exhibit themes:”
 - **Eco-Garden / Sustainable Showcase** – contains various green technology elements to teach people and promote sustainability, including a bio-retention pond, wind turbine, water monitoring exhibit, solar power generators, etc.
 - **Coast Guard Station** – open-air gardens in a ruin-like setting that tells the story of the Coast Guard Station throughout the various spaces that remain.
 - **Ship Graveyard** - Open green space with historical boats scattered about for public viewing and interaction.
 - **Maritime Sculpture Garden** - Open green space with maritime sculpture pieces (anchors, small boats, etc.) for public viewing and interaction.
 - **Natural Habitats** – Potential for various habitats to be developed to allow visitors to learn about and experience different environments.
 - **Observation Tower** – Potential opportunity for the public to experience exceptional panoramic views.

SCHEME 3 CREATE AN EDUCATIONAL PASSIVE PARK WITH MONUMENTAL RUINS OF THE COAST GUARD STATION

Other Site Components

- **Boat / Bike Rental Hut** (566 sf)
 - Activity / recreational center at the end of the causeway open to the public for bike, paddle boat, jet ski, kayak, roller blade, sailboat, small boat, etc. rental.
 - Provides instructional access and training for boating and water activities for people who could not otherwise afford access to or have the opportunity to experience boating.
 - Serves as a boater concession stand, allowing boaters to temporarily dock at the site and pick up food, supplies, etc.
 - Possibility for community services for kids.
- **Water Taxi Stop**
 - Water taxi system linked to other water taxi stops, including a new stop at the Whiskey Island parking lot.
- **Public Boat Docking**
 - Allow public boat docking along the perimeter of the site and to the west of the causeway, creating a casual retreat and destination for boaters.

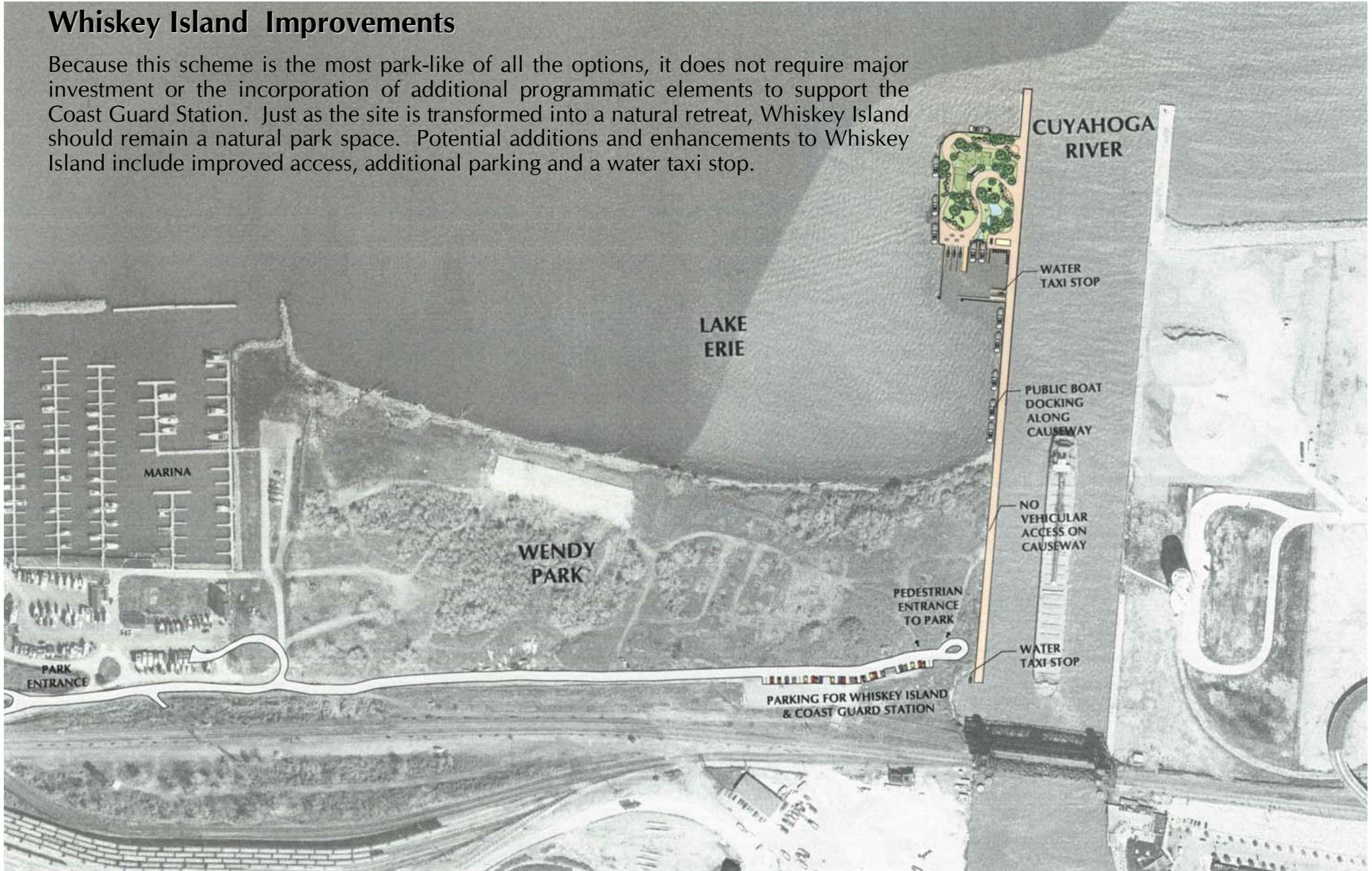
West Chelsea / High Line – New York
Abandoned elevated railroad transformed into a public park / promenade



SCHEME 3 CREATE AN EDUCATIONAL PASSIVE PARK WITH MONUMENTAL RUINS OF THE COAST GUARD STATION

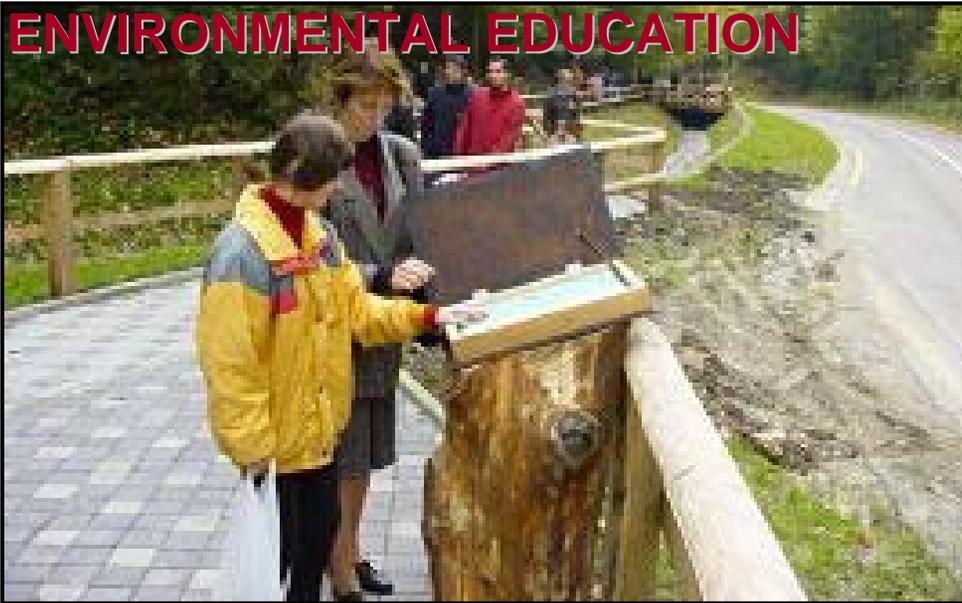
Whiskey Island Improvements

Because this scheme is the most park-like of all the options, it does not require major investment or the incorporation of additional programmatic elements to support the Coast Guard Station. Just as the site is transformed into a natural retreat, Whiskey Island should remain a natural park space. Potential additions and enhancements to Whiskey Island include improved access, additional parking and a water taxi stop.



SCHEME 3 CREATE AN EDUCATIONAL PASSIVE PARK WITH MONUMENTAL RUINS OF THE COAST GUARD STATION

ENVIRONMENTAL EDUCATION



SCHEME 3 CREATE AN **EDUCATIONAL PASSIVE PARK** WITH **MONUMENTAL RUINS** OF THE COAST GUARD STATION

PASSIVE PARK SPACE



SCHEME 3 CREATE AN EDUCATIONAL PASSIVE PARK WITH MONUMENTAL RUINS OF THE COAST GUARD STATION

ACTIVE PARK SPACE



SCHEME 3 CREATE AN **EDUCATIONAL PASSIVE PARK** WITH **MONUMENTAL RUINS** OF THE COAST GUARD STATION

ADVANTAGES

1. Smaller investment than other options
2. Supports the Waterfront District Plan, Ohio & Erie Canalway and Whiskey Island Park Plan
3. Creates a unique destination and park for boaters
4. Brings people to the water through a natural setting
5. Ties recreation, history, and education together through nature
6. Achievable with minor investment to Whiskey Island

& DISADVANTAGES

1. Original buildings and elements are demolished
2. Remote location raises surveillance & security issues
3. Compromises the original state of an Historic Landmark
4. Limited access – No vehicular traffic
5. Little possibility to generate revenue
6. Requires some entity to run the Activity Center

SCHEME 3 CREATE AN **EDUCATIONAL PASSIVE PARK** WITH **MONUMENTAL RUINS** OF THE COAST GUARD STATION

PROGRAM COST ESTIMATE

TOTAL ESTIMATED PROJECT DEVELOPMENT COST =

\$4,984,000

Scheme: Option Three - Create Monument							
Number	Item	Cost / SF	Cost / LF	Cost / Item	Units	Sub Item Total	Item Total
1 Site Components							
1.0	Demolition & Removal	\$0	\$0	\$600,000	1 LS	\$600,000	
1.1	Causeway @ 20'	\$0	\$775	\$0	1435 LF	\$1,112,125	
1.2	Site Utilities					\$50,000	
1.2.1	Green Self-sufficient Equipment	\$0	\$0	\$50,000	1 LS	\$50,000	
1.3	Pedestrian Pathways	\$1.75	\$0	\$0	12900 SF	\$22,575	
1.4	Landscape					\$81,000	
1.4.1	Topsoil /Seeding	\$0.87	\$0	\$0	20000 SF	\$17,400	
1.4.2	Trees	\$0	\$0	\$600	56 EA	\$33,600	
1.4.3	Planting Beds / Shrubs	\$3	\$0	\$0	10000 SF	\$30,000	
1.5	Story-teller Plaques	\$0	\$0	\$300	6 EA	\$1,800	
1.6	Public Boat Docking					\$127,200	
1.6.1	Slips	\$0	\$0	\$5,000	6 Slip	\$30,000	
1.6.2	Docking Along Causeway	\$0	\$300	\$0	324 LF	\$97,200	
1.7	Seating / Benches	\$0	\$0	\$500	12 EA	\$6,000	
1.8	Retaining Wall / Sheet Piling Replacement	\$75	\$0	\$0	15000 SF	\$1,125,000	
							\$ 3,125,700
2 Quarters Building							
2.1	Weather-proof Monument Structure					\$102,554	
2.1.1	Walls	\$8	\$0	\$0	9700 SF	\$77,600	
2.1.2	Floors	\$6	\$0	\$0	4159 SF	\$24,954	
2.2	Observation Tower	\$65	\$0	\$0	1000 SF	\$65,000	
							\$ 167,554
3 Bike / Boat Rental							
3.1	New Construction	\$100	\$0	\$0	566 SF	\$56,600	
							\$ 56,600
SUB-TOTAL - Construction Labor and Materials							\$ 3,349,854
Contractor's Overhead & Profit			15.0%			\$ 502,478	
TOTAL - Construction Cost							\$ 3,852,332
Contingency			15.0%			\$ 577,850	
Fees & Administrative Expenses			12.5%			\$ 553,773	
TOTAL - Estimated Project Development Cost							\$ 4,983,955

*Exhibit Space costs are not included in the above cost estimate.

SCHEME 3 CREATE AN EDUCATIONAL PASSIVE PARK WITH MONUMENTAL RUINS OF THE COAST GUARD STATION

CONCLUSIONS

Through this process, the planning team has attempted to identify a range of reality-based schematic design options with which to save the Coast Guard Station. Redevelopment concepts and program uses are reflective of and consistent with the public's objectives and goals that were generated throughout this process. By actively involving the community, this initiative resulted in a collaborative effort that encompasses the ideas, concerns and visions of the people that truly appreciate the value of this historic treasure to Cleveland.

As the condition of the Coast Guard Station continues to get worse, the need to intervene becomes greater, for once it is gone, this historic landmark and local gem is lost forever. As a result of this study, the fact remains that a considerable portion of the cost associated with the rehabilitation strategies lie with infrastructure rebuilding rather than with the restoration of the historic fabric. Therefore, throughout this process, a series of questions have arisen that encapsulate the primary challenges that the rehabilitation of the Coast Guard Station presents.

- Can we actually afford to save the Coast Guard Station?
- Can we truly afford NOT to save the Coast Guard Station?
- Where will the necessary resources come from to restore and sustain the property?
- Will the redevelopment of Wendy Park be enough to bring sufficient activity to warrant this considerable investment?

Because of a wide array of options for reuse and an ever-changing context which affects the site, this report is not meant to make any final conclusions. Instead, it can serve as the basis for next steps which would include investigating potential partners and funding sources to determine the future of the Coast Guard Station. First and foremost, resources are critical to focus towards protecting the building and site from deteriorating further and providing a safe place that is accessible to the public.

This report is meant to serve as a tool to raise awareness and inform people of the current state of the Coast Guard Station. Its mission is to identify the guiding objectives, generate ideas and spark possibilities which may lead to the restoration of this unique and important landmark building and site. This study has revealed that the true potential of the Coast Guard Station will resonate from the community's care, creativity and commitment that has guided and inspired this entire process.

Coast Guard Station Restoration

APPENDIX

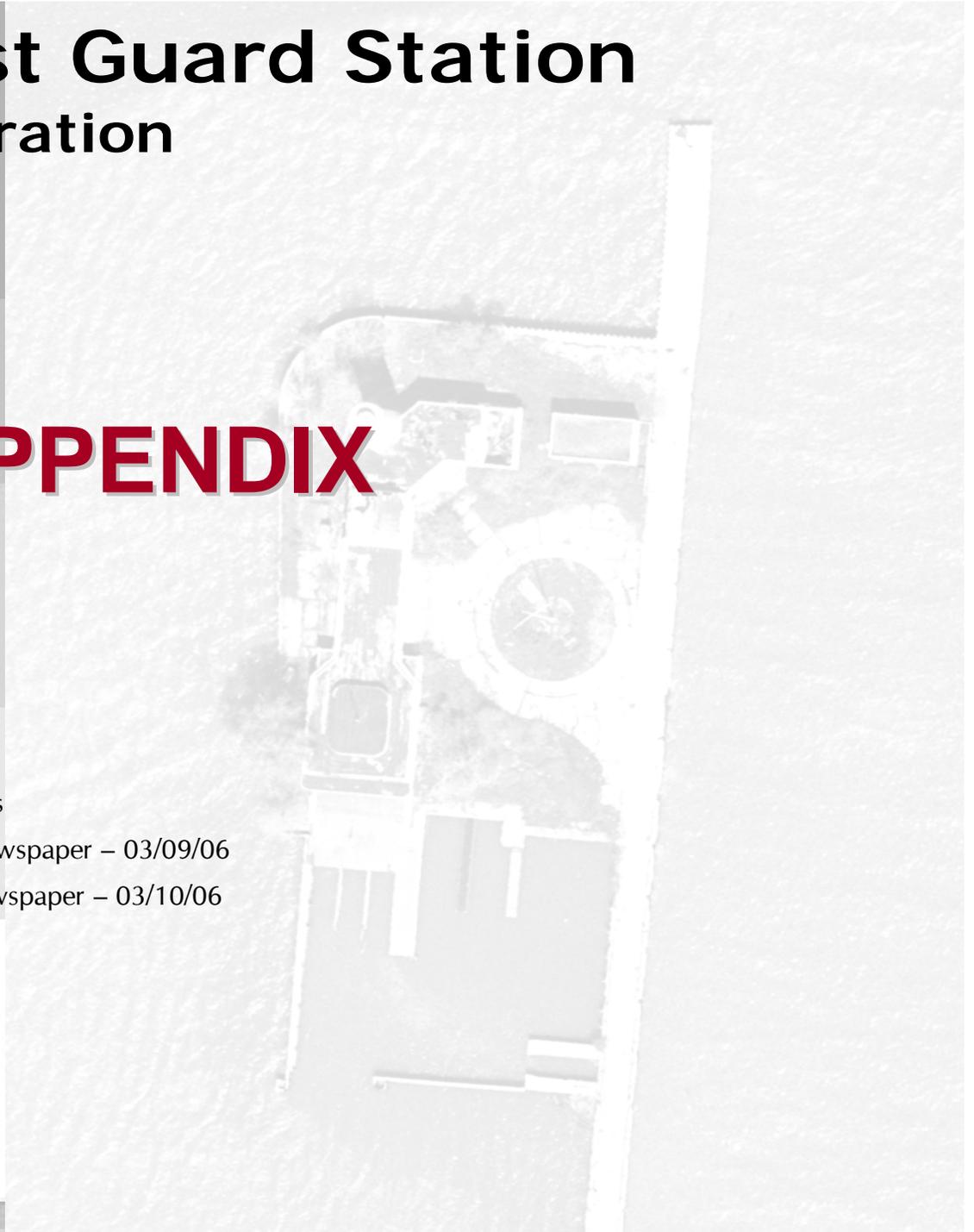
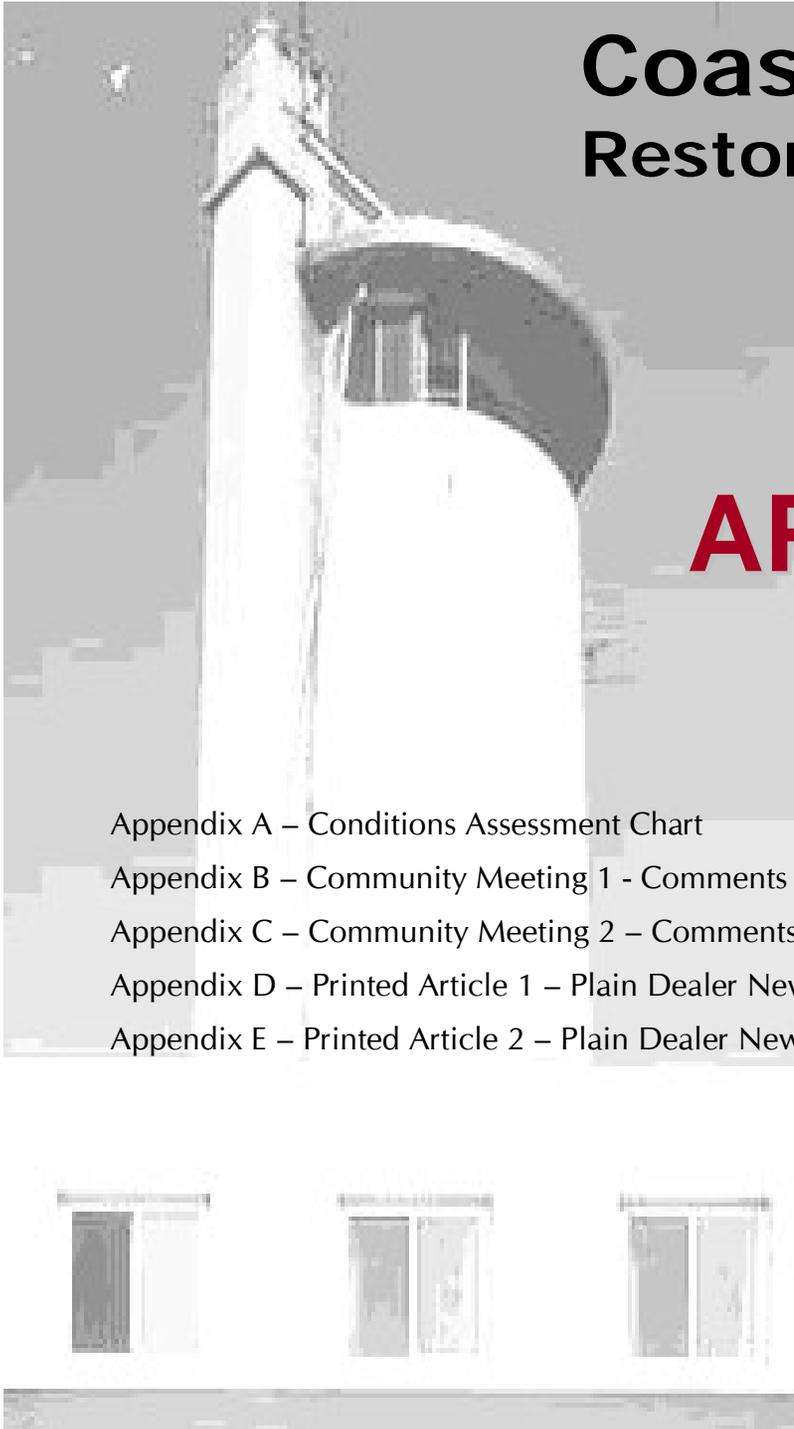
Appendix A – Conditions Assessment Chart

Appendix B – Community Meeting 1 - Comments

Appendix C – Community Meeting 2 – Comments

Appendix D – Printed Article 1 – Plain Dealer Newspaper – 03/09/06

Appendix E – Printed Article 2 – Plain Dealer Newspaper – 03/10/06



Appendix A: Conditions Assessment Chart

CLEVELAND HARBOR COAST GUARD STATION CONDITIONS ASSESSMENT

9.1 Assessment Chart

Original Construction	Current Condition	
2 Site Work		
Steel H Piles	Repairable	
Septic Tank System	Beyond repair	
Underground Ducts and Utility Structures	Beyond repair	
Subdrainage	Beyond repair	
Cement Concrete Pavement	Beyond repair	
Pavement Joint Sealants	Beyond repair	
Hot-Mix Asphalt Paving	Beyond repair	
Chain Link Fence and Gates	Beyond repair	
Lawns and Grasses	Repairable	
3 Concrete		
Reinforced cast-in-place concrete walls.	Repairable	
Reinforced cast-in-place concrete canopies.	Repairable	
Concrete slab at basement and first floor.	Repairable	
Concrete stairs to basement and mess hall.	Repairable	
4 Masonry		
Glass block windows at upper floor of boathouse.	Missing	
4" CMU walls on the inside of the concrete walls.	Repairable	
6" CMU interior walls for the basement and first floor.	Missing	
Flue lined chimney at tower.	Repairable	
5 Metals		
Steel rings embedded in concrete for ladder at tower.	Repairable	
Steel grating over the areaways.	Missing	
Steel columns and beams in boat house.	Repairable	
Steel trolley beam in boathouse.	Repairable	
Steel floor rails at boat launch.	Missing	
Steel beam at wall above mess hall.	Repairable	
Steel beam at center of garage.	Repairable	
Metal spiral staircase in tower.	Repairable	
Pipe railings at top of tower.	Beyond repair	
Pipe railings from first to second floor stair.	Beyond repair	
Sheet metal kitchen hood.	Missing	
6 Wood & Plastics		
Wood floor joists.	Beyond repair	
Wood roof joists.	Beyond repair	
Wood floor and roof decking.	Beyond repair	
Wood stairs.	Beyond repair	
Wood decking on boat house overhang and connecting roof.	Beyond repair	
Wood shelving in closets.	Missing	
¾" plywood soffits and ceiling in boathouse and connecting roof.	Beyond repair	
5/8" plywood walls in upper portion of boathouse.	Beyond repair	
Wood sills at boathouse clear story windows.	Beyond repair	
Wood trims.	Beyond repair	
Linoleum countertops and backsplashes.	Missing	
7 Thermal & Moisture Protection		
Copper lined gutters.	Missing	
Copper flashings.	Beyond repair	

CLEVELAND HARBOR COAST GUARD STATION CONDITIONS ASSESSMENT

9.1 Assessment Chart

Original Construction	Current Condition	
Asphalt asbestos built-up roofing.	Beyond repair	
Canvas covering to roof overhangs on boat house and connecting roof.	Beyond repair	
Metal decking on sun deck.	Beyond repair	
Waterproofing on concrete walls below grade.	Repairable	
Cork insulation board on roof (one inch).	Beyond repair	
Rock wool insulation in attic spaces above connecting roof.	Beyond repair	
Roof scuttle at top of tower.	Beyond repair	
8 Doors & Windows		
Pivoted steel sash windows with single pane glass.	Missing	
Steel upward action garage doors with glass.	Missing	
8 Doors & Windows Continued		
Wood entrance doors with glass.	Missing	
Wood doors with louvers at bedroom closets	Missing	
Wood screen doors at screened in porch.	Missing	
Kalamein clad doors at top of tower.	Beyond repair	
9 Finishes		
Painted exterior concrete walls.	Beyond repair	
Lath and plaster finish.	Beyond repair	
Tile window sills.	Missing	
Asbestos board ceiling in garage.	Missing	
Vinyl tiles.	Missing	
Linoleum flooring.	Missing	
10 Specialties		
Bronze letters as exterior signage.	Missing	
Fire hose cabinets.	Missing	
11 Equipment		
Gas range in Crews kitchen and Officers kitchen.	Missing	
Refrigerators in Crews kitchen and Officers kitchen.	Missing	
12 Furnishings		
Work benches in garage and garage storage.	Missing	
Wood cabinets in kitchen, mess hall, and offices.	Missing	
15 Mechanical		
Stainless steel kitchen sink.	Missing	
Stainless steel sink in garage.	Missing	
Laundry trays in basement.	Missing	
Water heater in basement.	Beyond repair	
Toilet, sink and metal partition in boathouse.	Missing	
Toilet, sink and shower in Guest Officers bathroom.	Missing	
Toilet, sink and bathtub in Officers bathroom.	Missing	
Toilets, sinks, urinals, showers, and metal partitions in Crew bathroom.	Missing	
Cast iron roof leaders.	Beyond repair	
Sheet metal exhaust hood in kitchen.	Missing	
Boiler heating with storage and expansion tanks.	Beyond repair	
Unit heaters in boat house.	Beyond repair	
Wall type radiator heating.	Missing	
16 Electrical		
200 amp service panel.	Beyond repair	
Original Construction		
Electric range outlets in Crews kitchen and Officers kitchen.	Beyond repair	
Vent fans in Crews kitchen and Officers kitchen.	Missing	
Incandescent light fixtures.	Missing	
Electrical outlets and switches	Missing	

Appendix B: Community Meeting 1 - Comments

Coast Guard Station
Community Meeting Comments
June 28, 2005

1. How close are Cuyahoga County and the City of Cleveland to coming to an agreement on land acquisition at Whiskey Island?
2. How much of Whiskey Island will remain park?
3. Why invest money at Coast Guard station if plan is to destroy Whiskey Island marina?
4. Why was a previous bond issue used to move iron ore to Whiskey Island?
5. Where will funding for Coast Guard restoration come from?
6. Likes idea of self-sustaining green place. Small rooms lend well to a youth hostel use if historic restoration is done. Wants trail to lead all the way to the Coast Guard Station (from Towpath). Bikes and walking, no cars. Parking at Whiskey Island, for proposed marina, will destroy area. Keep Port Authority where they are.
7. Has the Railroad seen the plans? Is it possible to connect to the south across the tracks?
8. How strict are the restoration requirements for National Registered property?
9. This is a great property. Should have multiple uses and functions going on, such as youth learning, classes, and educational programs.
10. Coast Station is one part of a three-part package, including Whiskey Island Park, Marina, and the Station. Seems to be a difference between the City and County's plans for Whiskey Island. Likes idea of Maritime Interpretation at the main structure (Quarters Building), restaurant or recreation use at the Boathouse and Garage. Suggests adding an additional exit spiral staircase at Observation Tower, to facilitate visitors.
11. Will casinos be a part of the Lake Front Plan? Where will (it) go?
12. Suggests a Maritime Museum such as the North Carolina Maritime Museum, Beaufort. Could also have Boat Education and Safety classes.
13. Has there been any discussion about connecting the Coast Guard Station to the Mather?
14. Suggests that any development at the Station be "maximum availability" and can be enjoyed by diverse groups of people. Feels Bed & Breakfast and youth hostel are restrictive uses. Feels restaurant has to be middle of the road, or a series of restaurants that offer a range of prices / are affordable. Also suggests a walkway from Edgewater Park, or a bus/transit service which would help Coast Guard Station uses stay open in Winter time.
15. Are there any plans for a 2nd (pedestrian) bridge over the Cuyahoga River to connect to the East Bank of the Flats? Also suggests using this bridge to connect to the RTA Waterfront Line. Should be a signature bridge.
16. Where can ideas be sent, after community meeting?
17. Tying into RTA line is excellent idea. Coast Guard Station would make great exhibit or museum use for Coast Guard history.
18. Is a restaurant use possible if National Register guidelines are followed for rehabilitation?
19. Recreate staterooms as originally designed, rent out as Bed & Breakfast. Also suggests a continuing education center (run by a college). Would like to see Coast Guard Station as "multi-use" and have something for all ages.
20. Can there be a marina use for transient boaters, and lodging or restaurant use? Overnight stays.
21. Tie into Cleveland Public Schools as part of a mentoring program / field trips. Connect the Coast Guard Station to education. County should be a partner.
22. Can docks be added at the causeway? Is there any additional dockage proposed? It would be a good way to get people there.
23. Tie back to park system with a bike trail, will help to create all of Whiskey Island as a destination.
24. Where is the bulk storage on Whiskey Island? What part of Whiskey Island is proposed to be a park?
25. Greater Cleveland Partnership should be involved with Coast Guard Station renovation, instead of being involved with Whiskey Island.
26. Breakwall adds unique feature to Whiskey Island: only place where you can sail right off beach. Should work community sailing program into the restoration. A new marina at Whiskey Island will destroy this opportunity.
27. Unrealistic to approach this project as a commercial use, should be sculptural only. Need to address quickly, seal the building as soon as possible. Demolition by neglect is occurring right now.
28. Need bigger idea than just a restaurant, this is unique spot on the water. Should be made unique to the region.
29. Port Authority and public bodies should not be dismissed as they are powerful entities which can help to make this happen. Should all work together and look past differences of opinion. Think about how to get public agencies to work for us and make our ideas happen. The missing pieces are the Port Authority and funding. Stop demonizing each other and thank the City for holding public meetings and listening.
30. There is confusion in the City's Lakefront Plan. It shows a new marina at Whiskey Island, which will add massive parking and infrastructure, which will ruin what is there now. A marina and a park are not the same thing.
31. Observation tower is the key. Seasonality is important, as the lake/site is nasty 7 months a year. The water around the site is not clean enough, but the opportunity here is to use the Station as a teachable monument. Transform the concept of "life saving" into "lake saving."
32. This site would make a wonderful community sailing center being right there next to a new park. There is a protected basin for launching centerboard boats – smaller lighter weight boats without large keels.

Appendix C: Community Meeting 2 - Comments

Coast Guard Station
Community Meeting Comments
March 9, 2006

1. Likes the first option the best where the Coast Guard Station is used as a museum. Suggests constantly changing the exhibits so that there is always something new for people to see, encouraging visitors to return to the site.
2. The amphitheatre / outdoor classroom is a great attraction to bring people to the site.
3. Shocked to see the Boathouse gone in the third option, not in favor of demolishing any of the buildings or parts because it takes away from the complete composition.
4. The Coast Guard Station could be an excellent application of hybrid cogeneration power, opening up possibilities for the site to create revenue or funding. This could also bring students to the site for demonstrations year round.
5. Remoteness of the site is a major problem, for many people have trouble finding Wendy Park. The towpath trail should tie into Wendy Park with a pedestrian bridge over the railway. However, even with a more direct pedestrian route to Wendy Park, there needs to be better accessibility for cars in order to work.
6. The more activities and programs introduced to the Coast Guard Station, the greater the demand for parking, which would take over Whiskey Island.
7. Suggests an east-west connection to Whiskey Island through a signature bridge across the mouth of the river, which would span 90 feet over the river in order to allow boats to pass underneath. This bridge could connect to the elevated tracks of the Waterfront Line, tying mass transit to the site through this "hands across the river."
8. How can Peter Lewis be involved in this investment?
9. The Maritime Museum in Vermillion, Ohio is currently 12,000 sf and needs to expand to 40,000 sf. Neither option provides enough space for a substantial Maritime Museum.
10. There are 8 other maritime organizations in Ohio, and the inclusion of an additional Maritime Museum in the Coast Guard Station may hurt the others.
11. Most museum visitors are older, and walking along the causeway to get to the Coast Guard Station might be scary for older people.
12. The Dawson Great Lakes Museum in Detroit is located in a park setting and is about to close.
13. Because access to the Coast Guard Station is so remote, only local people who know how to get there would come to the site.
14. The third option where the Coast Guard Station becomes "monumental ruins" is a ridiculous option. The restaurant proposed in the second option would cover the site's ability to sustain itself in the future.
15. Historic landmarks may be able to get money for stabilization, generally for roofs. The Coast Guard Station may qualify for this. However, a member of the Cleveland Landmarks Commission does not know of this grant.
16. Suggests cleaning up the site to get it so it is presentable, safe and stable. Allow people to go out there, promoting awareness and encouraging citizen power to save the Coast Guard Station.
17. Is the Metroparks going to take over Wendy Park? A representative of the Metroparks commented that if the towpath trail is connected there, they would be interested.
18. Crissy Field in San Francisco was sited as a model for raising money - \$34 million was raised by the Golden Gate Conservatory from mostly corporations in a grassroots effort. The creation of a conservatory might be the way to go.
19. Few places are as important as the Coast Guard Station. The public sector should take over the site in order to prevent revenue issues from driving the design and use of the site.
20. How will the Coast Guard Station survive in the winter? Who will deal with the ice and snow?
21. The Coast Guard Station may actually be busier in the winter because of school kids visiting the site for educational field trips. The Coast Guard Station should be an interpretation of Lake Erie and the ecology of the site.
22. Suggests using the Coast Guard Station as a Great Lakes Research Center, with potential to tie into Northeast Ohio's 3 Universities and giving them a presence on the Lake. It could also serve as an outdoor recreational destination for college students. Could money come from the Universities to help save the Coast Guard Station?
23. Ed Hauser – The Lakefront Plan needs to be revised because it currently shows a marina between Whiskey Island and the Coast Guard Station. The Coast Guard Station should become an environmental center, promoting green awareness in the very spot the river caught on fire. Currently, the restaurant 2,000 feet away from the site has enough trouble sustaining itself, so how could two restaurants in this remote location survive? The buildings need to be boarded up and the pier needs to be opened to get people out to the site to raise awareness.
24. A restaurant at the Coast Guard Station would be a great destination and attraction like Hornblowers or the Yacht Club's Restaurant. Boaters would love it and it would help generate money.
25. Suggests a dramatic modern glass shell to be built around the Coast Guard Station, creating a 21st century design.
26. Is it possible to immediately fix-up the site just enough to allow people to wonder around the site and go up to the observation tower?
27. The Non-profit Wendy Park Foundation is working to get more people out to Whiskey Island and wants to include the Coast Guard Station in its effort.

Appendix D: Printed Article 1 - Plain Dealer Newspaper – 03/09/06

Visions of an eatery and museum for abandoned Coast Guard station

TOM BRECKENRIDGE
Plain Dealer Reporter

The iconic, abandoned Coast Guard station at the Cuyahoga River's western mouth would be restored to glory — and multiple public uses — under multimillion-dollar plans that will be unveiled tonight.

City planners envision the 65-year-old station converted to a restaurant and Great Lakes museum, with public docks and a promenade.

The Cleveland Planning Commission will gather public comments on three versions of a restored station during a 6 p.m. meeting tonight at Gordon Square Arcade, West 65th Street and Detroit Avenue.

Estimated costs on the three proposals range from \$5 million to \$9.2 million. The city has no money for the work but believes plans for a restored site — it's on the National Register of Historic Places — would draw tax credits and federal, state and foundation funds.

"It's a critical piece of the city's lakefront and an amazing building," said Debbie Berry, the city's lakefront planner.

The city acquired the site two years ago. It remains in woeful disrepair, with a collapsed roof, rotting interior woodwork and 3 feet of water in the basement of the main quarters.

But the station's sleek, reinforced concrete shell is structurally sound, architects said last summer. The former Cleveland Harbor station was designed by the late J. Milton Dyer, whose many works include Cleveland City Hall.

The station features a main building and six-story tower that offers stunning views of downtown, as well as a boathouse and three-bay garage.

Plans include restoring the decrepit, 1,000-foot pier that links the station with Whiskey Island and the county-owned Wendy



PLAIN DEALER FILE

Cleveland is the proud, new owner of a coveted architectural site. Now the city must find millions of dollars to fix up the former Cleveland Harbor U.S. Coast Guard station.

"It's a critical piece of the city's lakefront and an amazing building."

Debbie Berry,
Cleveland's lakefront planner

Park. Ultimately, many see the station as the northern end point for the Towpath Trail, a bike path under construction now between Cleveland and Dover in Tuscarawas County.

Under an \$8.5 million plan, the site would be restored in historic detail, with a café and a museum devoted to Great Lakes and Coast Guard history.

Boats could dock along a perimeter promenade, while visitors on land would park on Whiskey Island and walk the pier, widened to 25 feet. A water taxi could squire visitors to the Flats or North Coast Harbor.

Under a \$9.2 million plan — called "full build" by Berry — the site would feature a restaurant, patio and banquet facilities, with a smaller museum.

Many more boat slips would be added, and a wind turbine is proposed for the circular driveway.

Docks would have room for a

"floating school" that would take students on the lake for science experiments, possibly in conjunction with the Great Lakes Science Center.

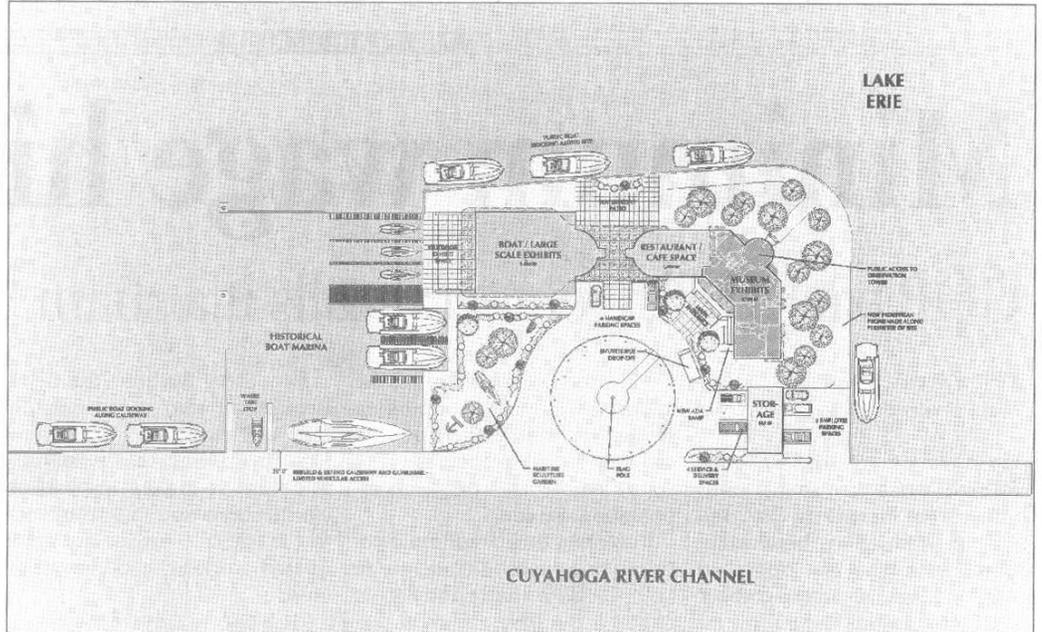
The site's northwestern tip would feature an amphitheater or outdoor classroom.

A third plan, costing \$5 million, would restore the building's shell but feature an open-air interior, surrounded by trees and walkways.

The advantage of a park-like setting is lower maintenance costs, Berry said.

Architect Paul Volpe, whose firm helped produce the plans, said something must be done soon to save a treasure that has become a lakefront "embarrassment."

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This rendering is one of three proposed plans for restoring the abandoned Cleveland Harbor U.S. Coast Guard station. This plan would return the site to its original design.

Appendix E: Printed Article 2 - Plain Dealer Newspaper – 03/10/06

Activists seek repairs fast for Cleveland's abandoned Coast Guard station

TOM BRECKENRIDGE
Plain Dealer Reporter

Lakefront activists called Thursday night for immediate repairs to the city's historic, but abandoned, Coast Guard station, an architectural landmark at the mouth of the Cuyahoga River that is rotting away.

A rapt crowd of 50 shook their heads and murmured at slides showing the battered, wind-swept station. They gathered at Gordon Square Arcade, West 65th Street and Detroit Avenue, to view the city's visions of what

a restored Coast Guard site would look like.

The 65-year-old station is three buildings — main quarters with observation tower, a boat-house and a garage — that sit at the west side of the mouth of the river, linked by a 1,000-foot causeway to Whiskey Island and a county-owned park.

One of the city's greatest architects, the late J. Milton Dyer, designed the building in a sleek Art Moderne style. It's on the National Register for Historic Places. But it's perilously close to being lost, experts said.

"The future of the Coast Guard station is in jeopardy," said architect and city consultant Courtney Lepene. "We need to act now to save this."

Much of the roofing is gone, and the site is choked with weeds and small trees. The causeway must be rebuilt, said consultant Geoffrey Varga, of Ralph Tyler Companies.

Planners have three distinct visions for the station. Under an \$8.5 million plan, the three-acre site would be restored in historic detail and feature a maritime museum, café and perimeter

walkways.

A \$9.2 million plan emphasizes commercial uses, featuring a restaurant, outdoor dining, banquet facilities and a boat-turned-classroom, besides a museum.

A third plan, costing just \$5 million, would convert the site to an educational park.

No one spoke in favor of the third option. Several spoke in favor of the restaurant and museum. Others said the difficulty in finding Whiskey Island, the station's remote site and the exposed walk along the causeway

make such uses questionable.

Several speakers called on the Cleveland Metroparks to take over the site. Jim Kastelic, a Metroparks planner, said the park system could take on the site only when it links with the Towpath Trail. But that's years in the making — the hike-and-bike trail stops six miles short of downtown.

City planners and consultants hope to craft a final plan in several months, and then stir interest with federal, state and non-profit agencies that fund the restoration of historic treasures.